

THE WORLD'S FAVOURITE AIRCRAFT MODELLING MAGAZINE

Scale AVIATION Modeller International

14
REVIEWS
INSIDE

**NEW
SERIES**
Modellers Profile
Scale Plans & Colour Art

**FREE
INSIDE**

A pair of
Thunderbolts
in 1/48

BRISTOL
Brabazon
REVIEWED

Part 8
16 page
guide

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F-14 TOMCAT

Squadron

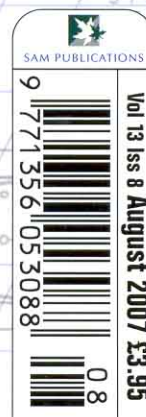
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Scaling down
F/A-18E
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2007

A-10 Warthog

- **Profile** - Fairey Swordfish
- Boeing V-22 Osprey
- P.111 Delta Prototype
- Lear Jet Fantasy



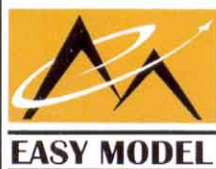
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Ugly But Well Hung

756 by David Francis

HobbyBoss 1/48. Both single and two-seater Warthogs
from the new tooling



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1/48 Hasegawa. A flight of fantasy with a spectacular finish



People and Places

748 A quick look at this year's Flying Legends at Duxford

As this is the issue we expect to have on sale at the IPMS US Nationals this year, it seems appropriate at this juncture to greet all our US readers, and if you have paused for a breather in your visit to the show and are reading this report in the vicinity of the SAM Publications stand, then please do wander over and say hello as soon as you have caught your breath and had a quick coffee – or whatever your particular pick-me-up may be.

If you are not fortunate enough to have been able to attend the show, then we hope to bring you an exhaustive report next month, but in the meantime we invite you to have a look through the latest news updates and marvel at the extent to which manufacturers all over the world seem to have gone to produce an absolute torrent of kits in time for August's main event. With E-day and Telford also looming closer, the pressure seems to be on to have ever more product on the tables. Good news for us modellers, of course, so keep an eye on these pages and we'll try to keep abreast of it all.

Closer to home, thanks to all the many readers who dropped by the stand at Duxford's Flying Legends weekend. It was a really international affair, and we hope the brief report elsewhere in this issue will whet your appetite for next year's show.

Now, on with the News.

MONTHLY PRIZE DRAW

Subscribers' Monthly Prize Draw

July

Mr Mattei, USA – Kit

Mr Cook, UK – Kit

Mr Carels, Belgium – Kit

Entry to the monthly prize draw is FREE to all subscribers.

Onwards and Upwards

PUBLISHER'S LETTER

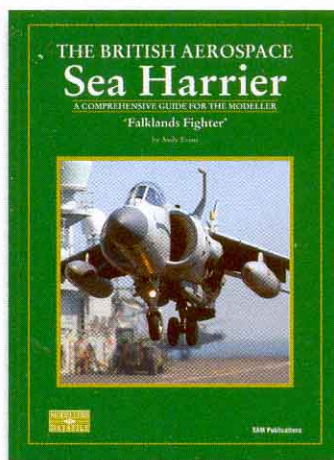
In the spirit of the company motto 'onwards and upwards' I am delighted to announce the following developments within Sam Publications.

From this issue, Neil Robinson has stepped up as Group Editor of the SAM stable of titles; *SAMI*, *Model Aircraft Monthly* and *Scale Models Collector*. Since joining SAM Publications, Neil has proven himself to be every bit as good as the reputation he has gained worldwide as the gregarious Yorkshireman with the encyclopaedic memory bank of military aviation. Our most recent readership survey showed that 60% of *SAMI* readers also now purchase *Model Aircraft Monthly*, demonstrating how Neil's editorship of this title has really paid dividends. We have therefore redesigned and repositioned both titles to compliment each other. For £8 per month you now have a 200-page one-stop solution to everything you need to know about your hobby, from news and reviews right through to historical reference.

I am similarly delighted to announce Gary Hatcher will now be working with Neil taking on the role of Editor of *SAMI*. I am sure it has not gone unnoticed that Gary has been taking a far greater editorial involvement in *SAMI* over the last 12 issues. Gary has worked with SAM Publications for many years now and in the last 4 years has been primed ready for the big job! For the last 2 years or so, we have benefited from the assistance of Mike Jerram as Managing Editor and Gary has learned much from a true professional. It has been a pleasure to work with Mike, and wearing my commercial pilot's hat, I have also enjoyed his work in the wider aviation press for many years. We look forward to a continued relationship with Mike.

Readers will notice some new features coming into *SAMI*. The *Modellers Profile* series will recognise the magazine's roll as a resource for

people who collect and build kits – combining scale plans, colour artwork, a kit review, historical notes and lists of available product. The series is unique in collating all the chief requirements in one concise article. Short and to the point, the *Profiles* aim to provide all you need to get started on a project, while recognising wider research is possible for those who feel the need. The series starts with the Fairey Swordfish, and looks forward to charting unusual territory in the coming months as well as



taking a closer look at selected aspects of some mainstream types.

New to this issue is veteran US columnist Steve Palmer, who will be providing us with regular helpings of his American Patrol – taking a look at aviation and modelling related aspects of American life and history. A number of new feature contributors have also been welcomed on board, and we look forward to presenting their work in due course, along with our usual up-to-the-minute coverage of news, kits, decals, accessories and other modelling products.

On the books front, *Modellers Datafile 11, The Sea Harrier*, is now out. *Modellers Datafile 13*, a unique look at the intended career of the TSR.2 based on archived documents, will be released

at the IPMS Nationals at Telford, and in between the two will come the first volume on the F-4 Phantom. This, *Modellers Datafile 12*, will cover USAF variants, and should be available in September.

There is no secret formula that can be applied or short cut utilised to ensure a product maintains its number one status. A reputation for quality and consistency cannot be achieved overnight, it must evolve over time. Since taking *SAMI* under my publishing wing some 13 years ago, I have spared little cost in building a worldwide team that is simply the best. Added-value free giveaway marketing campaigns such as last year's *Airfix AirKraft Guide* have rewarded our loyal readership and brought new modellers into the fold, both young and old. This year's Squadron MMD sponsored 12-part *World of Flight* guide has demonstrated commitment to our readership and indeed to the hobby in general. Our regular surveys tell us not only what you want, but just as importantly who, and where you are. We can't please all of the people all of the time but what cannot be ignored is the latest data from WH Smith in the UK, Barnes & Noble and Borders in the USA and other international outlets that conclude *SAMI* now sells four times more copies worldwide than any other static plastic model magazine!

Finally I am always open to suggestion for ways to improve our titles, and welcome your letters and emails. Even the words of the gentleman at the UK Nationals in 2005 who very firmly informed me "1/72 scale is disgusting" are important to me!

Steve Elliott
Publisher

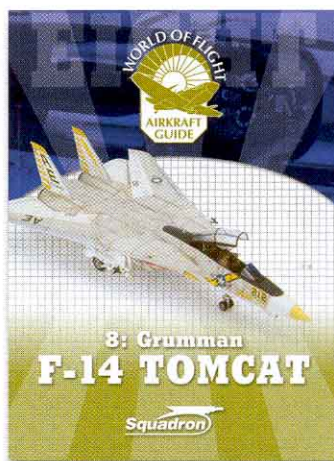
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Signals at Go!

SQUADRON

This month we include Part 8 of our new 12-part *World of Flight Aircraft Guide* giveaway, sponsored by Squadron MMD. This month's Guide is devoted to that perennial favourite of modellers, the Grumman F-14 Tomcat.

The *Guide* has been compiled in conjunction with our readers' survey, and is intended as a guide to modelling the top 12 most popular aircraft types as voted by yourselves. Taking the form of a digest of currently available kits and accessories, and illustrated not only with samples of merchandise, but also one of a series of specially commissioned features by modellers from 12 different



nations, the *Guide* constitutes a distillation of all the best from *SAMI*'s

review pages with respect to the aircraft concerned.

Aimed at model builders, rather than collectors, each part will help the modeller to identify what is current in the marketplace, and given their association with all the best products around, across the whole spectrum of the hobby, we can't think of a better sponsor than Squadron with which to have it linked. ■

**Free next month – Part 9:
Messerschmitt Bf 109**

Squadron

Making the Most

MPM

Now that most of the early-war Fleet Air Arm types are out on general release, the mighty MPM works has turned its attention to another lamentably under-kitted type, and seems to have started a trend, as all of a sudden there are more Pe-2 kits flying around than Gannets – which is no small achievement at the moment!

Three 1/48 kits are either out, or very shortly due out from MPM, although these releases are limited re-runs and readers are advised their availability may be fleeting.

- 48041 Petlyakov Pe-2 FT £29.55
- 48045 Petlyakov Pe-2UT Trainer £28.50
- 48046 Petlyakov Pe-2/Pe-3 £28.50

1/72 modellers need not lament the



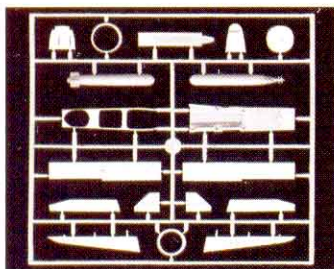
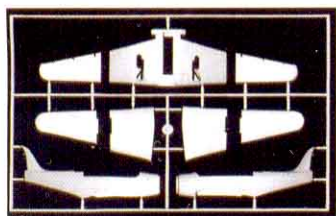
continuing unavailability of the Airfix kit, as their chosen scale is catered for by Unimodel, whose two issues are reported elsewhere in these pages. ■



See you Later Vindicator

OLIMP

A very welcome release from Olimp, and a nice addition to sit beside all those yellow-winged biplanes is the 1/72 injection moulded Douglas TBD-1 Devastator (#72007/TBA). With three kits of the type available from Valom, and one from Pavla, fans of the early-war US Navy are having a field day. But then, with over 100 kit releases in the recent period, so are we all. ■

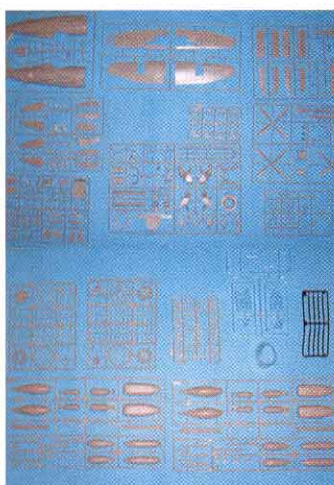
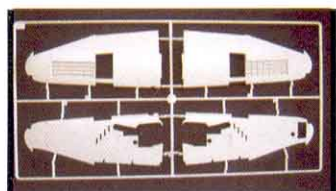


Big Deal

TRUMPETER STOP PRESS

Thunderbolt Test Shots

Trumpeter 1:32 P-47D Thunderbolt Test Shots have been popping up, and although no release date is imminent, it cannot be far off judging by the examples seen of late. ■



At a Glance

NEW IN AUGUST

This month sees another huge crop of kits, with over 100 finding their way into circulation. Many will be re-issues, or simply re-releases, but with the Trumpeter F-100D, the HobbyBoss 1/48 Super Hornets, Special Hobby's Roc and Skua and even a new Airfix kit finding its way into the list, this will have to be remembered as a particularly good month for modellers in all scales.



Airfix	#05115	1/48	Supermarine Spitfire Mk.I	£10.99
AJ Model	#72001	1/72	PZL W-3T Sokol Polish helicopter	£13.40
AMC Models	#4811	1/48	Zlin Z-326 T 'Trenar Master'	£34.50
AML	#72043	1/72	Nakajima Ki 43-II KAI	£12.99
Anigrand Craftwork	#7273	1/72	Myasishchev M-17 Stratosfera	£38.30
Attack	#14412	1/144	MiG-21 SMT	£5.99
AZ Models	#72014	1/72	Aero Ae-1	£12.55
AZ Models	#72023	1/72	Avro 621 Tutor K3241	£14.30
AZ Models	#72024	1/72	Avro 621 Tutor K6118	£14.30
Azur	#2872	1/72	MD 312 Flamant	£17.70
Classic Airframe	#4114	1/48	Dornier Do 17Z in Finnish Service. Alter	£42.50
Classic Airframe	#4121	1/48	Avro Anson Mk I Post War markings	£37.50



Czech Model	#3201	1/32	Brewster F2A-3 Model 339-23 Buffalo	£59.99
Czechmaster	#1172	1/72	Hawker Typhoon Mk IB early version	£23.99
Czechmaster	#1178	1/72	Spitfire Mk VII/HE VII	£26.99
Dragon	#4582	1/144	Tornado F3	£8.50
Dragon	#5032	1/72	P-38 Pathfinder	£16.50
Dragon	#5552	1/48	Heinkel He 162D	£23.99
Eduard	#8192	1/48	Avia B-534 IV serie	£15.50
Eduard	#8495	1/48	Mirage IIIC 10-RF	£11.75



FE Resin	FER14479	1/144	Mirage IIICJ	£15.75
FE Resin	FER14455	1/144	Mitsubishi Ki-46-II/II Kai Dinah	£18.99
FE Resin	FER14456	1/144	Mitsubishi Ki-46-III Dinah	£18.99
FE Resin	FER14457	1/144	Mitsubishi Ki-46-III Kai Dinah	£18.99
FE Resin	FER14458	1/144	Mitsubishi Ki-46-IV Dinah	£18.99
FE Resin	FER14461	1/144	Piasecki H-21 Flying Banana	£17.50
FE Resin	FER14475	1/144	McDonnell F-101B Voodoo	£19.15
FE Resin	FER14476	1/144	Saab J-29B Tunnan	£15.75
FE Resin	FER14477	1/144	CS-199	£13.65
FE Resin	FER14478	1/144	P1101	£13.65
FE Resin	FER14480	1/144	Sabb J-29F Tunnan	£15.75
F-rsin	#4461	1/144	Martin 202 - Allegheny	£29.99
F-rsin	#4465	1/144	Martin 404 - TWA/Southern	£29.99



Hasegawa	#00838	1/72	RF-4E Phantom II 'AG51 Immelman Special'	£17.99
Hasegawa	#00839	1/72	B-25J Mitchell 'Foreign Air Force'	£23.99
Hasegawa	#00841	1/72	F-15E Strike Eagle 'Iraqi Freedom'	£16.99
Hasegawa	#00843	1/72	AD-6 Skyraider 'VA-85 Black Falcons'	£13.99
Hasegawa	#00844	1/72	F/A-18F Super Hornet 'Low Visibility Par'	£13.99
Hasegawa	#00845	1/72	Mitsubishi Karigane Type 1 Communication	£13.99
Hasegawa	#09735	1/48	A-4SU Super Skyhawk 'Singapore Air Force'	£19.99
Hasegawa	#09736	1/48	F/A-18E Super Hornet 'Low Visibility'	£32.99
Hasegawa	#09737	1/48	TF-104G Starfighter 'Boelcke Special'	£17.99
Hasegawa	#09738	1/48	P-400/P-39D Airacobra 'Pinup Girl'	£16.99
Hasegawa	#09739	1/48	Messerschmitt Bf 109G-6 'Eyeball'	£16.99
Hasegawa	#09740	1/48	P-40N Warhawk '15,000th Anniversary'	£16.99
Hasegawa	#09741	1/48	Nakajima Ki-27 Type 97 Nate 'Nomomghan Incident'	£16.99
Hasegawa	#027	1/32	P-47D Thunderbolt Bubbletop	£29.99

continued over...

NEW IN AUGUST

...continued



Hobby Boss	#80317	1/48	Dassault Rafale B	£16.99
Hobby Boss	#80319	1/48	Dassault Rafale M	£16.99
Hobby Boss	#80321	1/48	F/A-18C Hornet	£25.99
Hobby Boss	#80322	1/48	F/A-18D Hornet	£25.99
Hobby Boss	#87214	1/72	Eurocopter EC-665 Tiger	£8.99
Hobby Boss	#87216	1/72	UH-60A Blackhawk Gulf War 1991	£8.99
HR Model	#7368	1/72	Skoda Kauba S.K.257	£16.80
Italeri	#1018	1/72	Junkers Ju 88A-4	£9.99
Italeri	#1264	1/72	Lockheed F-117A Nighthawk	£7.50
Italeri	#1266	1/72	Dornier Do 217-1/J-2 Nightfighter	£9.99
Italeri	#2666	1/48	Sikorsky MH-60K Blackhawk SOA	£12.99
JMGT	#20	1/48	Aviaon L.E. (Laboratoire Eiffel)	£44.60
LF Models	#48001	1/48	Ambrosini SAI.207 + BMPD	£42.85
LF Models	#72083	1/72	Fiat G.50bis	£30.60



Magna Models	#7287	1/72	Bristol Type 148	£19.35
Mirage	#48133	1/48	PZL P.23 Karas	£29.99
MPM	#48041	1/48	Limited re-release Petlyakov Pe-2	£29.55
Ozmods	#14404	1/144	T-33 Shooting Star USAF and Japan	£9.99
Planet Models	#13448	1/48	Seafire Mk.45	£44.60
Planet Models	#16548	1/48	Hughes H-1 Long wing version	£32.15
Planet Models	#19448	1/48	SAI-Marchetti SF-260	£30.35
Proteus Models	#008	1/72	Canadair CC-109	£71.99
Proteus Models	#009	1/72	Convair 440 S/N (pug radar)	£68.50
Proteus Models	#010	1/72	Convair 440 (long nose)	£68.50
Revell	#4209	1/144	Airbus A340 AUA 'Wiener Philharmoniker'	£14.99
Revell	#4252	1/144	Lockheed L-1049G Super Constellation	£12.99
RVHP	#7181	1/72	Swearingen SA 227TT	£45.99
South Front	#72001	1/72	Mil Mi-26	£47.50



Special Hobby	#32015	1/32	Nieuport 11 Bebe	£25.70
Special Hobby	#48046	1/48	Blackburn Skua Mk.II	£20.99
Special Hobby	#48050	1/48	Blackburn Roc Mk.I	£20.99
Special Hobby	#48066	1/48	Fiat G.55 'Centaur'	£19.99
Special Hobby	#72036	1/72	Fiat Cr.25 twin engine fighter	£16.70
Special Hobby	#72117	1/72	Heinkel He 100V-8 'World Speed Record'	£11.20
Special Hobby	#72156	1/72	Messerschmitt Me 264 with BMW 801 engine	£33.80
Special Hobby	#72157	1/72	Mitsubishi Ki-83 'Tokyo 1946'	£19.30
Trumpeter	#02232	1/32	F-100D Super Sabre	£79.99
Trumpeter	#02255	1/32	Grumman F4F-3 Wildcat (Early)	£29.99
Trumpeter	#02260	1/32	Messerschmitt Me 262A-1a	£42.99
Trumpeter	#06233	1/350	F/A-18C Hornet	£4.99
Trumpeter	#06234	1/350	F/A-18D Hornet	£4.99
Trumpeter	#06235	1/350	F/A-18F Super Hornet	£4.99
Trumpeter	#06236	1/350	F-14B/D Super Tomcat	£4.99
Trumpeter	#06237	1/350	Grumman EA-6B Prowler	£4.99
Trumpeter	#06238	1/350	Grumman C-2 Greyhound	£4.99
Trumpeter	#06252	1/350	Fairey III	£4.99
Unimodel	#101	1/72	Petlyakov Pe-2 (serie 1)	£13.70
Unimodel	#109	1/72	Petlyakov Pe-2 (serie 205)	£13.70
Valom	#72023	1/72	Polikarpov TIS (A)	£17.60
Valom	#72029	1/72	Kugisho D3Y-2K/D5Y1	£11.25
Welsh Models	#7211	1/72	Grumman C-20G/H U.S.A.F./U.S. Navy	£35.20
Welsh Models	#146P	1/144	Convair CV-440 SAS Viking 1	£25.70
Welsh Models	#149V	1/144	H.P. Herald 200 British United/BUA	£14.99
Welsh Models	#152P	1/144	Avro 748 Srs 1 Skyways Coach	£21.30
Welsh Models	#158V	1/144	Lockheed L-1011 TriStar 500 BWIA	£37.25
Welsh Models	#170P	1/144	Douglas DC-7C SABENA	£34.10

Shopping Lists

CMR

Always nice to see a good 'forthcoming products' list – especially when it is issued by such a prolific manufacturer. CMR's current programme makes satisfying reading:

1/72nd

- CMR-5096 Supermarine Spitfire Mk.21 (upgraded model)
- CMR-5126 Supermarine Scimitar F.1 (new payloads: Bombs, Missiles, Drop Tanks & Weapons Pylons)
- CMR-142 Blackburn Buccaneer S.Mk.1/NA.39 [Pre-Production]
- CMR-149 Fokker C.V.E Finnish Version
- CMR-151 Douglas Dolphin RD-3/RD-4
- CMR-158 Blackburn Firebrand TF Mk.IV/5/5a/TT.5
- CMR-161 Boulton Paul P.120
- CMR-162 Midget Mustang (racer)
- CMR-163 Avro York C.2 (complete model)



- CMR-166 De Havilland Vampire F.1 (new model)
- CMR-167 De Havilland Vampire F.30/FB.31
- CMR-171 Cessna 150
- CMR-173 Piper PA-18/L-18 Super Cub
- CMR-176 Supermarine Spitfire TR.9 Twin-Seat Trainer version
- CMR-177 Supermarine Spitfire Mk.IX/Mk.XVIE Foreign Users
- CMR-179 Supermarine Spitfire F/LF/HF Mk.VIII in RAAF Service - 'Special Part 1' July 2007
- CMR-180 Supermarine Spitfire F/LF/HF Mk.VIII in RAAF Service - 'Special Part 2'
- CMR-181 Hawker Typhoon NF Mk.IB Night Fighter Prototype



- CMR-182 Boeing Model 40B-4 (new model)
- CMR-183 Republic RC-3 Seabee (new model)
- CMR-184 Supermarine Spitfire Mk.IXC

1/72nd Helicopters

- CMR-146 Bristol Sycamore HR.14/Mk.52
- CMR-174 MMD 900 Explorer

1/72nd Gliders

- G-29 Laister-Kaufman TG-4 July 2007
- G-30 Pratt-Read LNE-1 July 2007
- G-31 LET L-13AC Blanik (TG-10C Kestrel) July 2007
- G-32 LET L-23 Super Blanik (TG-10B Merlin)

1/144th Mini Master Series

- CMM-010 Fokker F.VIIB/3m Trimotor
- CMM-011 Lisunov Li-2 Civilian versions
- CMM-012 Lisunov Li-2 Military versions



1/72nd Conversion & Detailing Sets

- CMR-164 Avro Lancaster B Mk.II conversion set
- CMR-DS03 F-4E (S) Shablood conversion set
- CMR-DS04 Blackburn Firebrand TF Mk.5 Folding Wings detailing set
- CMR-DS05 Catapult Launch detailing set (Royal Navy)

Pet Shop Boys

FREIGHTDOG

Some late news from Freightdog. Following the recent decal issues they will be releasing a 1/72nd Spitfire FR Mk.18 large rudder conversion as a separate item in resin to go with the 32 Sqn option on the sheet. It also includes the retractable tail wheel that is missing from the Fujimi kit. This has been mastered by Alistair Mclean of A2Zee Models (who also did the Gannet wing fold sets under his Alley Cat range) and will be ready by the end of July.

Next decal release (in 1/72 and 1/48) will be 'Weekend Warriors - the RAuxAF 1948-53,' including a Spitfire LFXVie, Harvard T2B, two Meteor F8s and a Mosquito T3 to be released in August.

Freightdog will also be stocking (exclusively in the UK) the Nostalgic Plastic 1/144 resin C-133 mastered by Anigrand. More of this dainty anon... ■





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See you soon!

Navy Daze

TRUMPETER

Two new toolings look set to land any day now. In 1/72 we are pleased to see the Hawker Sea Fury. 93 parts make up the kit, which includes a folding wing option and markings for WJ232, 802 Sqn HMS Ocean 1952, and WE683, 1831 Sqn 1953.

The second is another naval aircraft – the 1/32 F6F-5NHellcat. Like the F6F-3N, the F6F-5N was meant as a night-fighter. The aircraft were fitted with AN/APS-6 radar, and armed with two 20mm cannon and four 50 cal. machine guns. Flown by both the Navy and Marines from carriers and land bases, other than the radar installation and different armament the F6F-5N was essentially the same as the standard F6F-5.

The model comprises 374 plastic parts, an etched fret and an optional clear engine cover.

Just to refresh our memories, here are some of the items listed in Trumpeter's 2007-2008 Catalogue that have not yet put in an appearance:

1/24

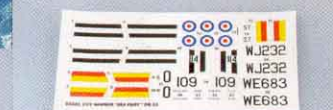
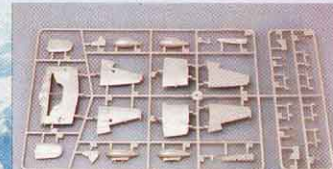
- P-51B/C
- Fw190A-6/8
- Ju-87D Stuka

1/32

- Curtiss P-40E Warhawk
- Curtiss P-40M Kittyhawk
- Curtiss P-40N Kittyhawk
- Douglas TBD-1 Devastator
- AV-8B Harrier Early Version
- F-100F Super Sabre
- Grumman F8F-1 Bearcat
- Grumman F8F-1B Bearcat
- Grumman F8F-2 Bearcat
- A-6A Intruder
- A-6E Intruder
- Grumman F6F-3 Hellcat
- Grumman F6F-5 Hellcat



- Republic P-47D Thunderbolt Razorback
- Republic P-47D Thunderbolt Bubbletop



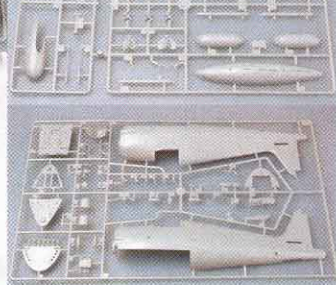
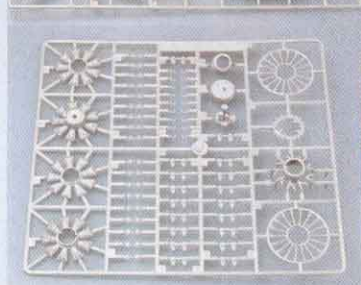
- A-4E Skyhawk
- Su-27UB Flanker C
- Su-30MKK

1/48

- Seahawk Mk.100/101
- Soviet Mig-3 Early

1/72

- Republic F-105D Thunderchief
- Republic F-105G Thunderchief
- CH-47A Chinook
- CH-47D Chinook
- Su-15 TM Flagon-F
- Su-15 UM Flagon-G
- Wellington Mk.IC
- Wellington Mk.III
- Wellington Mk.X
- Hawker Sea Fury Fb.11
- P-40B/C
- Chinese K-8 Karakorum Trainer
- Fw200C-3 Condor
- A-6E Intruder
- Su-27 Flanker-B
- Beriev Be-6 Madge
- Mig-19 Framer
- F-100D Super Sabre



Universal Rejoicing

UNIMODEL

Unimodel have a large and fascinating array of 1/72 armour available in injection moulded plastic, but to date their involvement with the aircraft side of things has been limited to some neat little kits of German anti-aircraft guns and the like.

Now, it seems, they have branched



out and – excuse the expression – spread their wings. First two aviation releases are the Petylyakov Pe-2 (serie 1) (#101/£13.70), and the Petylyakov Pe-2 (serie 205) (#109/£13.70). According to the boxart the kits feature around 170 plastic parts and 65 photo-etched, which will make them into very detailed models of this much-neglected Soviet dive-bomber. ■

Eastern Edition

TAMIYA

Readers will be aware of Tamiya's occasional re-packaging of Italeri kits. The Hs 123 was one such. Latest in the line is the Fiat CR42 (#89722/TBA). Tamiya has included a full set of accessories, including pilot figure, fuel drums, and bombs. 4 colour options are provided, including one from the Western Front, and the kit can be built with or without landing gear spats.



While this item is unlikely to be available on general release to the UK, there is every reason to suppose it will be obtainable by mail order from HobbyLink Japan, or some such source. ■





SIG News

SCALING DOWN IPMS

Interest in 1/144 is reviving, with a mixture of new kits from major manufacturers, cottage industry kits and underground small-scale producers. Though IPMS-UK has a successful Airliners Special Interest Group, it is some years since there has been a 1/144 scale group covering the whole range of smaller scale aircraft modelling.

David Hart of 11, Lodge Farm Close, Walton, Chesterfield, Derbyshire S42 7LF is working to form a SIG. The aim would be to exhibit at major



shows and to set up a web-based SIG with activities such as a Flickr group to share photos of members' models. Contact David if you are interested. The Editor already has! ■

Crop Circles

HASEGAWA

More limited edition re-issues from Hasegawa this month, including the postwar Lanc, and a couple of others in particularly nice schemes. Grab them now, or wait until they come round again...

- ST28 1/32 Kawasaki Ki61-I HEI Hien (Tony) £24.99
- 09747 1/48 AH-64D Apache Longbow J.G.S.D.F. £TBA
- 09749 1/48 F-104 Starfighter 'Red Baron' £TBA
- 09752 1/48 A-4M Skyhawk VMA-131 Diamondbacks £TBA
- 00850 1/72 Lancaster 'Postwar' £TBA
- 00852 1/72 Junkers Ju88C-6 Nachtgeschwader £TBA
- 00853 1/72 F/A-18F Super Hornet VFA-103 Jolly Rogers' £TBA
- 00854 1/72 Mitsubishi G4M1 Type 1 (Betty) 'Repulse attack' £TBA
- 00855 1/72 F-4J Phantom II 'CAG Bird' £TBA ■



Zerstorer So Far

EDUARD

Eduard report a successful show at Brno's ModelBrno 2007 event, at which their upcoming Bf 110 and Hellcat were part of their display. This will be good news to those waiting eagerly for the Bf 110, anxious that no untimely problems should delay such a key project.

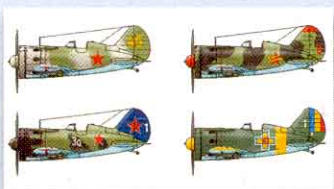
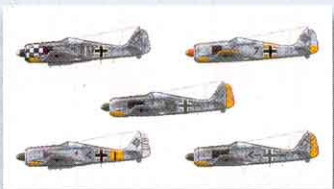
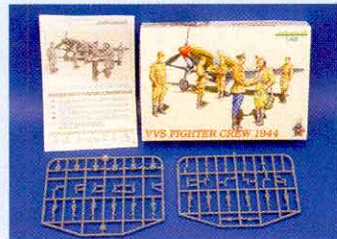
Kits due out immediately will kick off with the 1/48 Fw 190A-6 (#8171/£21.60), which will follow the pattern of previous releases, with five marking options in the box, a colour photo-etch sheet, a masking sheet and a comprehensive instruction booklet with full colour painting guide. All the pilots of the selected aircraft are well known aces, and Eduard point out that this version of the Butcher Bird was never properly released by any other manufacturer before.

The Mirage 2000D/N (#1123/£21.60) is another repacked kit in the Limited Edition line. The base kit is the Airfix/Heller release, with typical Eduard add-ons - a colour photo-etch sheet, resin parts (ejection seats) and new decals for four marking options, all for French AF aircraft. This item is not expected to be available for long, so if you want one, book early!



The VVS Fighter Crew, circa 1944 in 1/48th will be the next figure set. Eduard claim they will be issuing more figures through the remainder of the year, and have further sets ready to be released. This particular kit represents Russian fighter staff, in late WW II. The set contains six figures, as usual.

August will be also quite interesting with respect to Eduard kits. The new I-16 Type 29 will be released together



with the Hetzer tank destroyer Royal Class boxing. Not strictly within the remit of SAMI, but many of our readers build them all the same, the Hetzer Royal edition will be limited to 1000 pieces, featuring a single kit, but with bonus PE sets, metal barrel, a book by MBI, and a replica of the Iron Cross medal.

Besides these two main releases, there are two additional items; the Albatros D.V 1/48th scale in the Limited Edition line, and a 'US Navy Personnel 1942' 1/48th scale figure set, which latter will be a very useful addition to many modeller's collections.

Eduard are on final approach to the release of the 1/48 Messerschmitt Bf 110. They expect to have the plastic parts tested and verified by late July, and are threatening to send out samples to the editors of model magazines and websites in early August. Reviewers form an orderly line please! The expected release date is September 1st, but they are aiming to have the first version in boxes in late August to present this kit at the IPMS Nationals in Anaheim. The first version slated for release is the Bf 110E (# 8203/\$49.95), as this version has never been released by any other manufacturer, and is regarded by many as an important sub variant. ■

Scale Modelling News

OZMODS

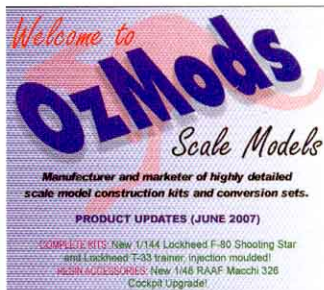
Ozmods Scale Models has taken over the marketing and distribution of SCALEDOWN products. The OzMod's web site will be updated to include the Scaledown products as soon as possible. ■

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Phone: + 61 + 7 + 3245 6509

E-mail: ozmods@bigpond.net.au

Web Site: www.ozmods-kits.com



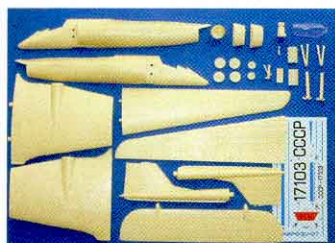
More Cold War Testbeds

ANIGRAND

No less than two brief lectures from Anigrand this month. Nice to see another manufacturer joining in with the 1/144 uprising too!

The first release, in 1/72, is a resin kit of the Myasishchev M-17 Stratosfera (#2073/£38.30), a Russian equivalent of the U-2. This was developed following the shooting down of Gary Power's U-2 spy-plane over the Ukraine, in the aftermath of which the Americans started to use alternative unmanned balloons that prevailing winds would carry across Soviet territory. The Soviet PVO (air defence forces) found this difficult to counter, and ordered Myasishchev to develop a high-altitude balloon-destroying aircraft. The result was the M-17 Mystic-A.

The first prototype was flown and crashed in 1978. The second prototype was constructed to a modified design, and first flown in 1982. After



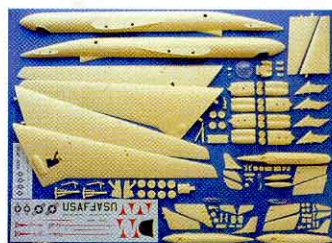
the US reconnaissance balloon program was superseded by spy satellites, the M-17 was modified as a test aircraft, and used for aerodynamic and atmospheric research. NATO gave it the code-name 'Stratosfera.' In 1990, it set 25

international speed/climb/height records. In 1992, it investigated the hole in the ozone layer over the Antarctic.

The M-17 fulfilled all its design

objectives, and proved successful in its politico-military role.

Scaling down to 1/144, the next



release is the Convair YB-60 (#4003/£51.20), whose fascinating story begins in 1941, when the USAAF began the Convair B-36 program, in anticipation of a need to conduct strategic bombing of Europe from the USA. However, the XB-36 maiden flight did not occur until 1946.

The USAAF wanted a faster design, so a specification for a new bomber to replace the B-36 was issued. Boeing responded with the Model 462, and

received a contract for two prototype XB-52s. As the B-52 was considered a high-risk program, the Air Force continued to support B-36 production until it was obvious that the B-52 would indeed work. In 1950, Convair submitted an unsolicited proposal for an all-jet, swept wing version of the B-36. The primary role was to secure a possible fall-back in case the B-52 ran into significant difficulties. The USAF was interested and authorized Convair to convert two B-36Fs to YB-60s, this type making its maiden flight in 1952. The overall performance was not significantly greater than the B-36, and in 1953, the USAF concluded that the B-52 would succeed in a timely manner, and the YB-60 effort was cancelled.

On a more conventional note, the 1/144th C-17s will be with us soon, in both RAF and US versions. ■

Another Galaxy Far Far Away

KIT HUNTING

Steve Palmer's American Patrol column focuses on the C-5 Galaxy in this issue. Rightly he alludes to the Editor's fondness for this machine, and given the generous response of readers to his plaintive request for assistance in tracking down a kit some months ago, it seems only fair to provide a brief update on the matter, while at the same time musing on the nature of essential kit purchases in general.

Thanks to the kind auspices of SAMI's readership, Mr. Hatcher had at one point no less than three 1/144 Galaxy's in his pending pile: a partially built Otaki kit with original box and decals, a boxed, mint Revell version, and an unboxed, but complete, kit minus decals and instructions. Having picked up some Dragon armour at Duxford to act as payload, one might suppose this would be enough for the fool, but not so...

This week on Ebay, a Testors kit in the coveted European scheme has popped up. So far Mr. Hatcher has bid unsuccessfully on kits by Otaki, Revell and Yodel – all for the same set of plastic. Now, unable to restrain the impulse, he is bidding again, and seems bent on expending a considerable sum on a kit he will never get round to building, but desperately desires for the

C-5A Galaxy



C-5A Galaxy



decal sheet alone.

The moral of this brief tale is simple. Next time a member of your family desires something that seems incomprehensibly stupid to you – a new three piece suite, a pierced belly button, or a tattoo of a lady with very little clothing – think long and hard on any follies you may be harbouring in your own breast before passing censure.

And for the sake of the Editor's bank balance, please don't bid on that Testors Galaxy! ■



It's a Miracle

MIRRACHROME

What's that car doing on the News pages of Scale Aviation Modeller International? Is nothing sacred?

Don't worry. It's all part of an interesting product availability to which we wanted to draw your attention. MirraChrome, the world's most realistic chrome-effect spray paint, with a reflectivity almost 98% that of true chrome plating, is now available direct in the UK through Autopaint International. Representing the latest in reflective coating technology, MirraChrome can be applied by standard spray gun or airbrush, then over coated with an automotive polyurethane varnish for a completely durable finish.

Autopaint International is one of the country's leading independent automotive refinishing products retailers, providing a one-stop-shop paints and sundries resource for smaller garages and body shops, custom bike and car builders, classic vehicle restorers, and most importantly, model makers. MirraChrome represents a genuine technological breakthrough, by virtue of its outstanding reflectivity rate and its capacity to be clear-coated without losing its authentic chrome look; it features a near-microscopic particle content, whose close alignment



produces superior surface orientation and an unequalled mirror-like shine.

Other metal finishes include Ghost Chrome, which comes close to replicating the look of brushed aluminium; the patented ChromeFX professional system, that offers a viable alternative to real metal plating; MultiChrome, which combines a sprayable chrome paint with an intense colour-change effect, when viewed from different angles; and MirraColour, a coloured sprayable chrome paint that can be topcoated almost immediately.

Now those Trumpeter F-100s are piling up, waiting for a metal finish, some modellers might like to look into the range of products on offer. Further information is available from Autopaint International, at www.autopaintfx.com or if you don't use a computer, on 0121 327 3713. ■

Back on Track

VINTAGE FIGHTER SERIES

Following a couple of setbacks to the project, VFS are back on track and you should see the first kit of the P-47 Thunderbolt D-25 around September/October 2007. In the meantime here is boxart for *Big Ass Bird 2* - a D-30 which will be released shortly after the first two D-25 versions. ■



Can Openings

JAGUAR

Another new, and bound to be classic, colour scheme. One can almost hear the artists at Model Alliance cursing as they labour away in the scriptorium. Pictures by Gary Parsons. ■



Big Wings

WELSH MODELS

Latest releases from Welsh Models in 1/144 are:

- SL 165 V - MD-11 in Delta Air Lines Blue red & white intermediate scheme £35.45
 - SL 180 P - Boeing 757/300 (RR) in American Trans Air £33.50
 - SL 181 P - MD-90-30 in EVA latest livery scheme £32.45
 - SL 183 P - Fokker F.27 500 Series in Channel Express (A300 scheme) £20.25
 - SL 184 V - Lockheed Tristar 100 (Early Engines) in TWA Twin Globe scheme £35.45
 - PJW 29P - Avro Vulcan B. Mk.2 (Price not finalised)
 - MT 18P - Bristol 170 Mk 31 Royal Canadian Air Force £26.25
- Parts are almost completed for a 1/72nd Boeing 737 next generation 600/700/800 range, which will first



be released in the Australian Air Force 700/7ES wedgetail AWACS version. This will be followed by the 700 civil version hopefully towards the end of the summer. Liveries are not yet confirmed.

Patterns being worked on in 1/144 include the Curtiss C-46, C-119 Fairchild Packet, and a EMB-170 / 190 /195. Liveries are to be announced. On the new pressure moulded resin front, new patterns have been completed for the Fokker



50 and FH227 and are with the resin manufactures. On the 1/144 military side, the Vulcan Bomber parts are likewise with the resin company and decals are expected shortly.

Welsh Models will also be releasing a series of 1/144 scale Boeing 707 derivatives, including a new KC-135R and the E-6B, all these new kits will have pressure moulded major parts. ■

Raise you Five

SPECIAL HOBBY

Big news from Special Hobby for this month is the 1/32 P-39D Airacobra (#32002/TBA). This well kept injection-moulded ace in the hole will be among a number dropped on the table at Anaheim in August, in anticipation of which your Newsdesk reporters will be bringing a very large suitcase and no spare changes of underwear.

Plenty of other items of interest from this direction though. The 1/48 Skua and the Roc are both out and proud, as is the 1/72 Heinkel He 100V-8 'World Speed' (#72117/£11.20). Don't forget that the He 100 is on the 1/32 future releases list from this manufacturer as well.



Other new kits include:

1/48

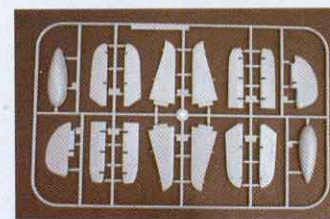
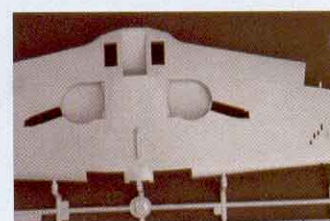
SH48066 Fiat G.55 Sotoserie 0

This kit represents the first version equipped with one cannon and four machine guns and the older style fin. It contains four sprues with grey plastic parts and one sprue with clear parts. The gun sight is resin cast and further details are offered as photo-etched parts. Decals offer one Reggia Aeronautica and two ANR aircraft.

1/72

SH72088 Nardi FN.305 Luftwaffe and Royal Hungarian Air Force 1/72

A re-issue of this interesting trainer, this



time with Luftwaffe markings.

SH72146 F-86K Sabre Dog NATO All Weather Fighter

The kit contains three sprues with grey plastic parts, vacform canopy, resin wheels and photo-etched parts. Decals are for three machines, all of them in natural metal finish. Both Italian and Dutch aircraft wear a unit marking on the vertical fin, while the most striking is the Norwegian aircraft that sports unit markings, shark mouth and an artistic drawing of a dog. Specific stencils are included for each of the three aircraft. ■



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Many Happy Returns

MONTEX

Two new 1/32 resin kits are due soon from Montex – better known for their masking sets: a Gloster Gamecock, and a Commonwealth Boomerang, both very welcome subjects in this format and scale.

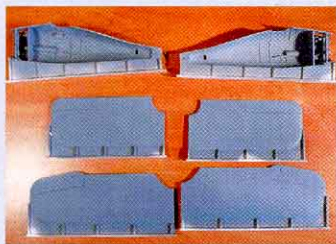
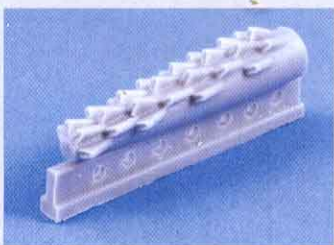
by Jay Laverty

There's no getting around the fact that the Boomerang does not enjoy the status of a Bf.109, Spitfire or Mustang, which is why we have not seen it kitted in anything other than vac-form before now in 1/32. Montex have pushed the boat out with this release, showing not only a willingness to take a risk on more esoteric subject matter, but also an



insight into what potentially has the hallmarks of a successful release.

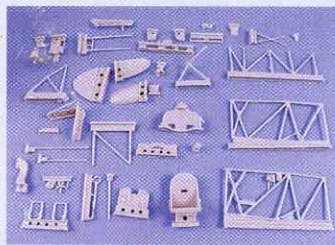
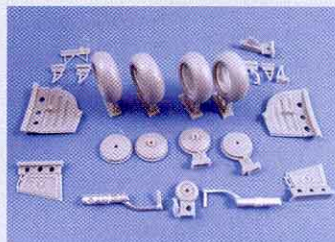
The Boomerang sparkles with personality, its distinctive lines giving it a



character all its own, thus making it potentially a very appealing modelling subject to many.

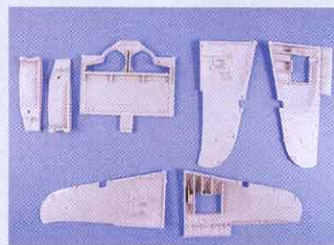
This kit immediately displays a thoroughness, and attention to detail that demand admiration. The construction layout is as logical and convenient as any injection moulded kit, and the instructions are superbly done. Better still, to avoid confusion, this model has all of the parts collected in zip lock bags pertaining to their stages of construction.

The casting is fantastic. Each highly

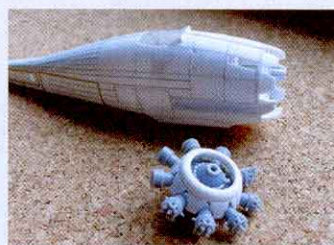


detailed part will require a minimum of effort to clean up, and the moulding has obviously been done very carefully, as there are no noticeable parts suffering from slippage. In particular, the tubular framework of the cockpit is perfectly round, and from my initial assessment should only require going over with a toothbrush to tidy things up. The entire cockpit is a highly detailed affair, and as a central focus area on any aircraft, it scores very highly on this kit.

The vacform canopy has the sections moulded separately, which I think is another unusual and very welcome consideration on behalf of Montex. The final few stages of construction incorporate the addition of the wings, prop, and flaps as well as the cowlings flaps, which are nicely done in photo-etched. There are essentially three versions of airframe included in this kit, as the exhausts are provided for the CA-12, CA-13 and 19.



All in all this is one very impressive debut aircraft release from a company with a well established reputation for quality. The assembly is straightforward, logically laid out, and should make for a nice choice for a first full resin kit, for anyone wishing to take the plunge. Showing a remarkable attention to care and quality, this is a promising start for Montex and one that hopefully will see a return in sales that will see them releasing more kits in the future. ■



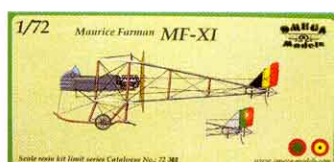
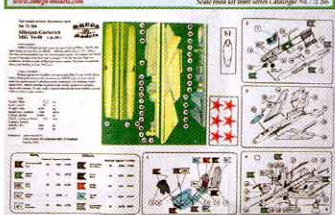
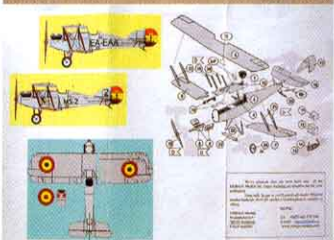
Kits Keep Coming

OMEGA

More releases have been announced from this Czech source. Kits are resin, with various multi-media parts. Particularly nice are the wheel sets for the Farman, which seem to include separate tyres and spoked centres.

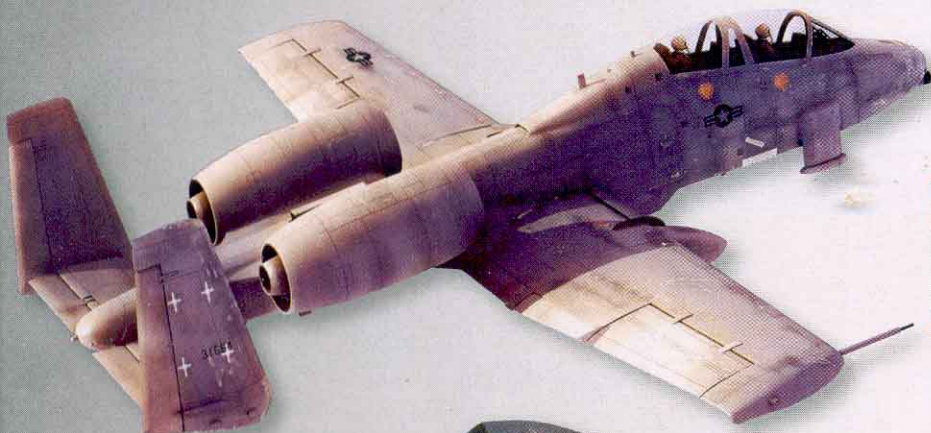
1/72 models announced include:

- Mikoyan Ye-50 Jet and rocket engines (#72266/£50.70)
- R.E.P. 1912 Floats (France) (#72288/£34.40)
- Maurice Farman MF-XI (#72300/£38.40)
- Martinsyde F-4A (#72307/£34.50)
- Martinsyde F-4 Zagorski (#72308/£34.40)
- Nieuport Ni-XXI (#72312/£38.40) ■



HOBBY BOSS

HOBBY BOSS

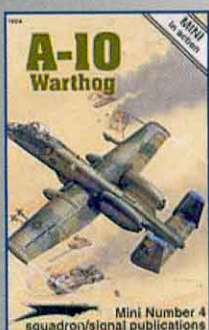


N/AW A-10 Thunderbolt II 1:48 Scale

US two-seat, twin-engine jet close air support aircraft kit, night/all-weather configuration. Decals for NAW A-1073-1664, Edwards AFB.

HY80324 ~~\$69.99~~ **\$62.99**

Squadron Signal



A-10 "Mini" in Action (SC)

Fondly known as the Warthog by its pilots and ground crews, A-10s saw widespread combat during the Gulf War, knocking out an impressive number of Iraqi vehicles. Over 50 photos, 25 detail drawings, 2 pages scale drawings, 10 full-color paintings. 50 pages.

SS1604 ~~\$5.95~~ **\$5.36**



A-10 Thunderbolt II 1:48 Scale

US single-seat, twin-engine jet close air support aircraft kit. Decals for 3 aircraft: 23rd TFW, 1990; 906th TFG, 23rd TFW, February 1991; 510th FS, 52nd FW, 1992.

HY80323 ~~\$69.99~~ **\$62.99**

HOBBY BOSS

Rafale B French Fighter 1:48 Scale

French twin-engined, delta-wing, multi-role fighter kit, two-seat variant. Decals for the Armée de l'Air.

HY80317 ~~\$59.99~~ **\$53.99**

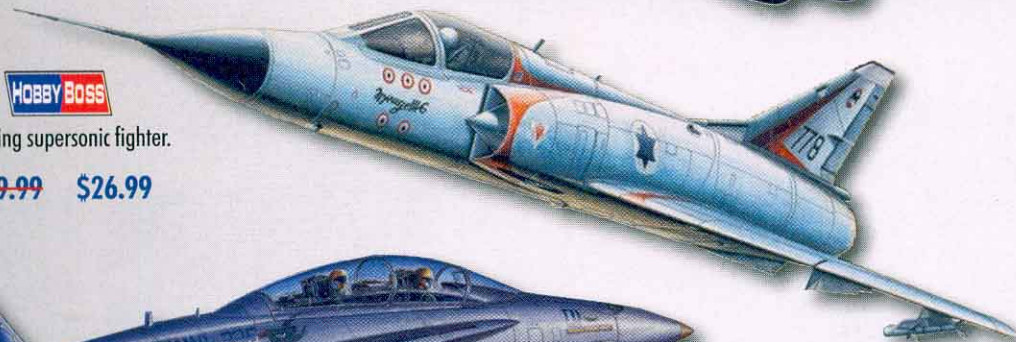


Mirage III CJ 1:48 Scale

French-built delta-wing supersonic fighter. Israeli version.

HY80316 ~~\$29.99~~ **\$26.99**

HOBBY BOSS



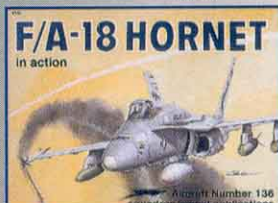
F/A-18D Hornet 1:48 Scale

US-built modern all-weather carrier-capable strike fighter kit, two seat variant. Includes decals.

HY80322 ~~\$69.99~~ **\$62.99**

HOBBY BOSS

Squadron Signal

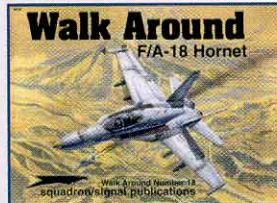


F/A-18 Hornet in Action (SC)

Chronicles the development, armaments, and variations of the F/A-18 Hornet. Includes full color artwork and profiles, black and white drawings and over 90 black and white photos. 50 pages.

SS1072 ~~\$12.95~~ **\$11.66**

Squadron Signal



F/A-18 Hornet Walk Around (SC)

The F/A-18 serves as a front-line fighter and strike aircraft on board the USN's largest fleet carriers and was used landing gear, engines and weapons. Color profiles, black and white line art and color paintings. 80 pages.

SS5518 ~~\$16.95~~ **\$15.26**

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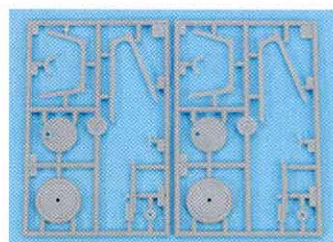
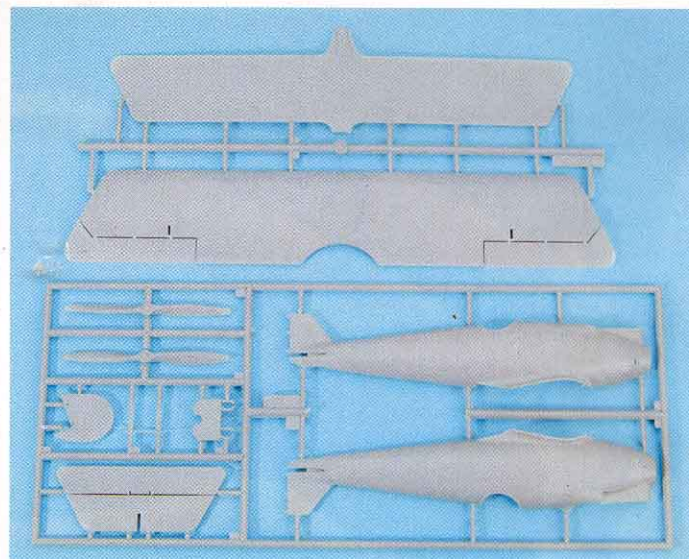
Scale Aviation Modeller International has a policy of previewing all kits received in this section, and any item received is guaranteed a mention at the earliest opportunity. We cannot realistically preview kits unless we actually receive them, and manufacturers are invited to submit samples as early as possible to ensure the most efficient usage.

Special Preview – D.III



Roden's 1/32 Pfalz D.III

Scale: 1/48	Kit No: 4114
Price: £36.17	Decal Options: 3
Panel Lines: Scale: 1/32	Kit No: 613
Price: £34.99	Decal Options: 5
Panel Lines: Recessed	
Type: Injection Moulded Plastic	
Parts: 81 Plastic, 1 Acetate sheet	

operational aircraft. This is an inspired choice by Roden.

The kit is very cleanly moulded in a tan-coloured plastic and areas like the trailing edges of the wing are very thinly moulded, so thin, in fact, that if you hold the sprues up to the light the plastic is translucent.

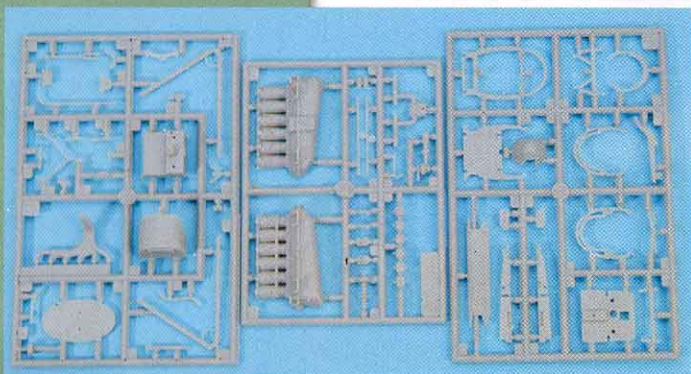
The engine section is almost a model in itself taking up almost a quarter of the total parts count and this and the nicely detailed cockpit will draw the eye of any one looking at your model. I am sure in due course Eduard will produce a set for this model but by adding seat belts most modellers will be happy with

the moulded detail supplied by Roden.

The decal options supplied in the box include four colourful machines with personalised insignias. The most colourful is the one illustrated on the box art, in a blue and red scheme with a large silver moon on the side. There is a fifth option which is in a much more subdued camouflage scheme of dark green and purple over light blue.

World War one subjects are perfect in 1/32 scale. Big enough that you can see all the detail but small enough that most modellers can find a home for one without the need for major rebuilding work. This model should make a superb introduction to larger scale models and even - rigging the normal downfall of many biplane models - should be relatively simple in this larger scale.

UK importers for Roden kits are Pocketbond. Squadron are US importers, and it is through their good offices that we have been able to take this early look. A full build review will follow in due course.



The latest in Roden's World War One subjects is perhaps the prettiest of German aircraft, the Pfalz D.III, a type that entered service during the summer of 1917 when German aircraft were carrying some of the most outlandish colour schemes ever applied to an



Supermarine Spitfire F.IX

Scale: 1/72	Kit No: 175
Price: £25.99	Decal Options: 6
Panel Lines: Recessed	Status: Revised Tooling
Type: Resin	
Parts: Resin 46, Vacform Clear 4, Etched 43	
Manufacturer: CMR	UK Importer: Hannants




Arado Ar 234 B-2/N Nachtigall

Scale: 1/48	Kit No: 04505
Price: £19.99	Decal Options: 4
Panel Lines: Recessed	Status: Revised Tooling
Type: Injection Moulded Plastic	
Parts: Plastic 147, Clear 10	
Manufacturer: Revell	




Lockheed P-38 Pathfinder

Scale: 1/72	Kit No: 5032
Price: £16.50	Decal Options: 2
Panel Lines: Recessed	Status: Revised Tooling
Type: Injection Moulded Plastic	
Parts: Plastic 54, Clear 1, Etched 10	
Manufacturer: Dragon	
UK Importer: The Hobby Company	





MIG-21 SMT

Scale: 1/144 Kit No: 14412
 Price: £5.99 Decal Options: 2
 Panel Lines: Recessed Status: New Tooling
 Type: Injection Moulded Plastic
 Parts: Plastic 38, Clear 1, Resin 1
 Manufacturer: Attack UK Importer: Hannants

Product from this company just gets better and better. Quite apart from having some of the finest panel lines yet seen in this scale, this latest release has a one piece resin cockpit tub that

even features detail on the instrument panel. Stores include one-piece wing tanks, and missiles, and there is an extra pair of wings included, and some additional hardware, which is indicative of, hopefully many, MIGs to come.

The canopy – very often the making or breaking of a 1/144 kit – is clear, well moulded and looks like an acceptable fit.



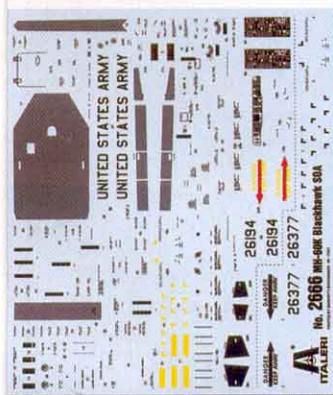
MH-60K Blackhawk SOA

Scale: 1/48 Kit No: 2666
 Price: £12.99 Decal Options: 3
 Panel Lines: Recessed Status: Revised Tooling
 Type: Injection Moulded Plastic
 Parts: Plastic 157, Clear 16
 Manufacturer: Italeri
 UK Importer: The Hobby Company



Dassault Rafale M

Scale: 1/48 Kit No: 80319
 Price: £16.99 Decal Options: 2
 Panel Lines: Recessed Status: Revised Tooling
 Type: Injection Moulded Plastic
 Parts: Plastic 147, Clear 4
 Manufacturer: HobbyBoss
 UK Importer: Creative Models

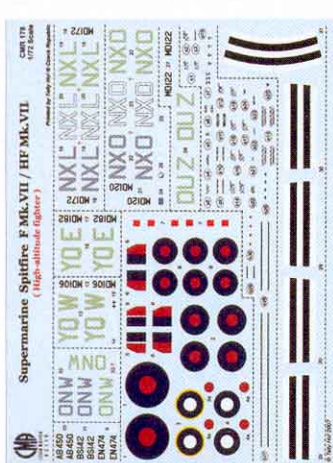


Supermarine Spitfire F/HF Mk. VII

Scale: 1/72 Kit No: 178
 Price: £26.99 Decal Options: 9
 Panel Lines: Recessed Status: Revised Tooling
 Type: Resin
 Parts: Resin 45, Vacform Clear 4, Etched 45
 Manufacturer: CMR UK Importer: Hannants

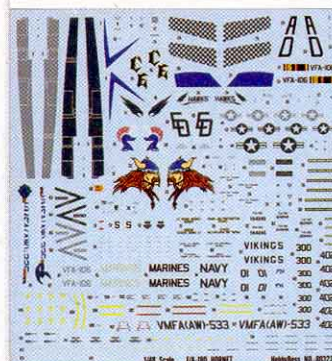
Another comprehensive Spitfire package from CMR. This includes an Eduard fret, with pre-painted instrument panel and cockpit parts, a

set of Kabuki masks and a massive decal sheet including nine options, stencilling and invasion stripes.



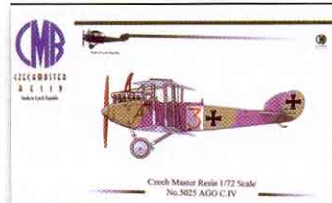
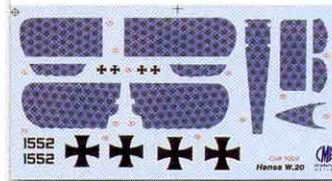
F/A-18D

Scale: 1/48 Kit No: 80322
 Price: £25.99 Decal Options: 3
 Panel Lines: Recessed Status: Revised Tooling
 Type: Injection Moulded Plastic
 Parts: Plastic 213, Clear 10
 Manufacturer: HobbyBoss
 UK Importer: Creative Models



Hansa W.20/2

Scale: 1/72 Kit No: 5005
 Price: £17.30 Decal Options: 1
 Panel Lines: Recessed Status: Revised Tooling
 Type: Resin
 Parts: Resin 52
 Manufacturer: CMR
 UK Importer: Hannants



AGO C.IV

Scale: 1/72 Kit No: 5025
 Price: £20.20 Decal Options: 3
 Panel Lines: Recessed Status: New Tooling
 Type: Resin
 Parts: Resin 47
 Manufacturer: CMR
 UK Importer: Hannants

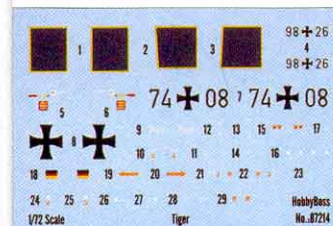


Eurocopter EC-665 Tiger (German)

Scale: 1/72 Kit No: 87214
 Price: £8.99 Decal Options: 2
 Panel Lines: Recessed Status: New Tooling
 Type: Injection Moulded Plastic
 Parts: Plastic 70, Clear 1
 Manufacturer: HobbyBoss
 UK Importer: Creative Models

It is time we all stopped thinking of HobbyBoss in terms of 'Easy Kits.' The latest releases received from UK Importer Creative Models are quality injection moulded kits that are comparable to the very best from Revell, Italeri or Hasegawa. The easy kits have been popular, both with younger modellers and others, but items like this crisp little Tiger go way beyond them in terms of detail and moulding.

There are new kits on the block! For all of us.



Dewoitine D.510

Scale: 1/72 Kit No: 72065
 Price: £15.50 Decal Options: 4
 Panel Lines: Recessed Status: New Tooling
 Type: Injection Moulded Plastic
 Parts: Plastic 41, Resin 40, Vacform Clear 2
 Manufacturer: Pavla UK Importer: Hannants



Special Preview – F-100

by David Francis

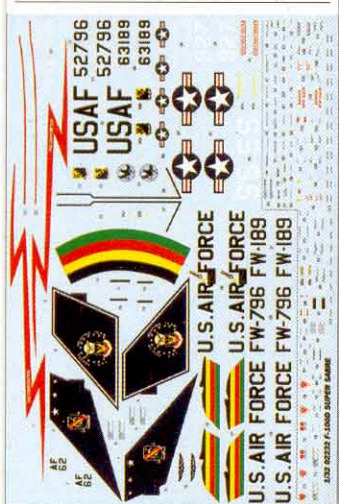
This kit was first announced two years ago and has been eagerly awaited by large-scale modellers ever since.

On opening the box we are presented with a generous number of plastic sprues, and on initial examination things look very promising. The most prominent feature is the large number of rivets. These are far more restrained than on previous releases, but are still a bit over the top – especially if you plan to complete your model in a natural metal finish. One area that really cries out for resin replacement are the underwing undercarriage bays, as these are moulded as part of the lower wing, and this has led to a very shallow appearance.

As supplied this model appears to represent an aircraft in service in the 1950s as an interceptor with the small 275 gallon fuel tanks, twin sidewinder rails and early style instrument panel, however the kit also includes some later features like the tail hook and optional bent refuelling probe.

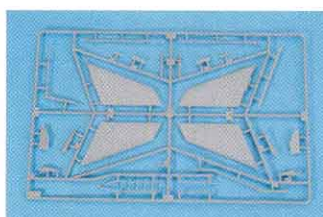
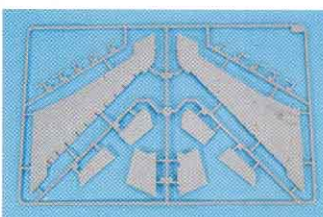
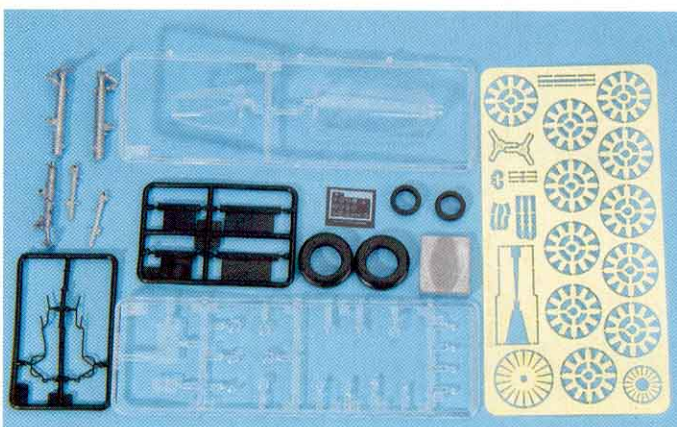
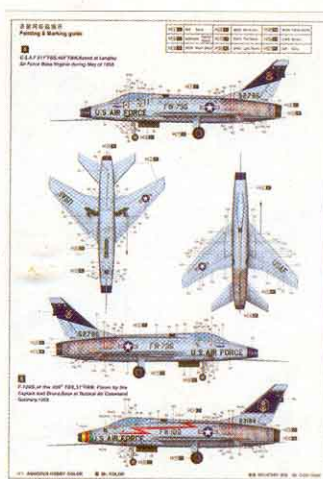
Like many Trumpeter kits there are areas that seem to be detailed just for the sake of it, like the 12 photo etched fan blades that fit inside the engine, but for some this detailed engine and separate tail section and mounting trolley will be welcomed for their diorama potential.

Now for the real nice touches like the long Pitot tube under the nose, which is provided in either the extended flight position or for the first time in model form with the probe folded up in to the vertical position which is how it is seen on most flight line photographs. The choice of either white metal or plastic undercarriage legs is to be applauded as



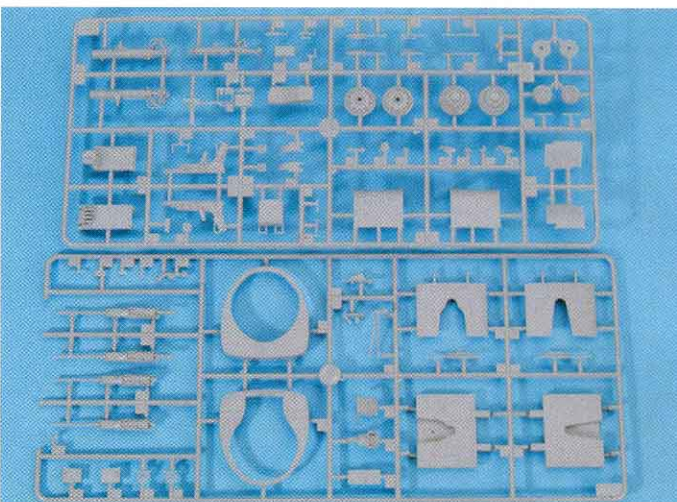
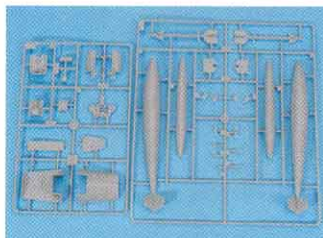
is the addition of a large weight that fits inside a compartment in the nose, and the open gun bays that again add to the diorama potential.

The decal sheet is typical of a Far East product, being very thick, with some



dubious translations in the stencilling, but I am sure by the time you read this there will be a large selection of aftermarket decals available to choose from.

With every kit Trumpeter seem to improve and the F-100 is no exception, and though I have a few small quibbles, this kit will build in to an impressive addition to your model collection.



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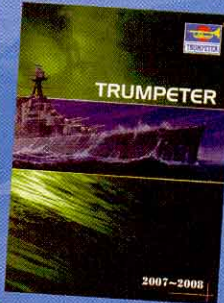
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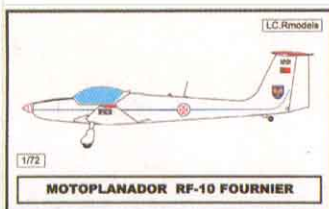
Portuguese Specials

Portuguese manufacturers LC Rmodels have sent us a couple of samples that warrant a closer look. Both are resin kits with decals and well-printed colour instruction sheets. The types covered are unusual, and will be of immense interest to many readers looking for something a little different – especially as the quality of the casting is so good. Some air bubbles are present on the



wing leading edges, but the detail seems crisp and well defined.

No UK distributor is currently indicated, but interested parties should contact lc.rmodels@gmail.com



Moto Planador RF-10 Fournier

Scale: 1/72 Kit No: N/A

Price: TBA

Decal Options: 1

Panel Lines: Recessed

Status: New Tooling

Type: Resin

Parts: Resin 12, Vacform

Clear 1

Manufacturer: LC Rmodels



Vickers Valparaiso I G.E.A.R.

Scale: 1/72 Kit No: N/A

Price: TBA

Decal Options: 1

Panel Lines: Recessed

Status: New Tooling

Type: Resin

Parts: Resin 40

Manufacturer: LC Rmodels



Avia B-534 IV Serie IV

Scale: 1/48 Kit No: 8192

Price: £15.50

Decal Options: 4

Panel Lines: Recessed Status: Revised Tooling

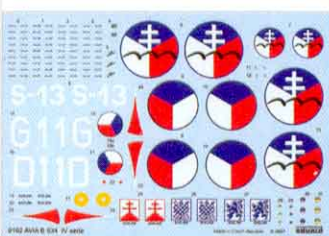
Type: Injection Moulded plastic

Parts: Plastic 91, Clear 5, Etched 54

Manufacturer: Eduard

UK Importer: Hannants/LSA

The second sitting of this kit is every bit as good as the first, and includes all the coloured etch, Kabuki masks, and lavish attention of the first.



Dassault Rafale B

Scale: 1/48 Kit No: 80317

Price: £16.99

Decal Options: 2

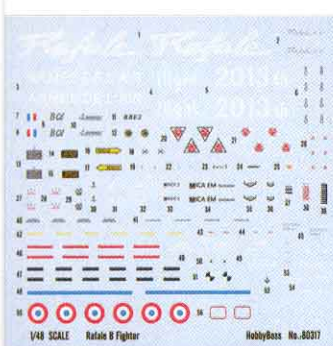
Panel Lines: Recessed Status: Revised Tooling

Type: Injection Moulded Plastic

Parts: Plastic 147, Clear 4

Manufacturer: HobbyBoss

UK Importer: Creative Models



F/A-18C

Scale: 1/48 Kit No: 80321

Price: £25.99

Decal Options: 2

Panel Lines: Recessed Status: Revised Tooling

Type: Injection Moulded Plastic

Parts: Plastic 213, Clear 10

Manufacturer: HobbyBoss

UK Importer: Creative Models



UH-60A Blackhawk

Scale: 1/72 Kit No: 87216

Price: £8.99

Decal Options: 2

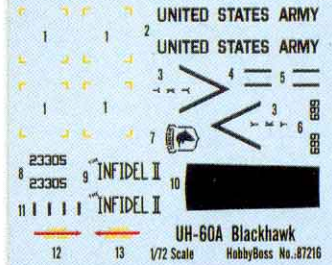
Panel Lines: Recessed Status: New Tooling

Type: Injection Moulded Plastic

Parts: Plastic 78, Clear 7

Manufacturer: HobbyBoss

UK Importer: Creative Models



Bristol Type 148

Scale: 1/72 Kit No: 8772

Price: £19.35

Decal Options: N/A

Panel Lines: Recessed Status: New Tooling

Type: Resin

Parts: Resin 7, Metal 13, Vacform Clear 2

Manufacturer: Magna

Available from: 54 Farcroft Road, Parkstone Poole,

Dorset, BH12 3BQ

Contact magna.models@ntlworld.com



Supermarine Spitfire Mk. I

Scale: 1/48 Kit No: 05115

Price: £10.99

Decal Options: 2

Panel Lines: Recessed Status: New Tooling

Type: Injection Moulded Plastic

Parts: Plastic 75, Clear 14 Manufacturer: Airfix



Do-217 J1/J2 Nachtjäger

Scale: 1/72 Kit No: 1266

Price: £9.99

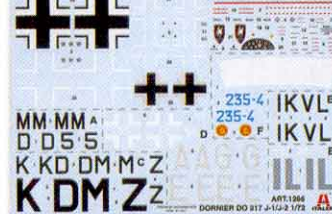
Decal Options: 6

Panel Lines: Recessed Status: Revised Tooling

Type: Injection Moulded Plastic

Parts: Plastic 99, Clear 4 Manufacturer: Italeri

UK Importer: The Hobby Company



Mirage IIIc

Scale: 1/48 Kit No: 8495

Price: £11.75

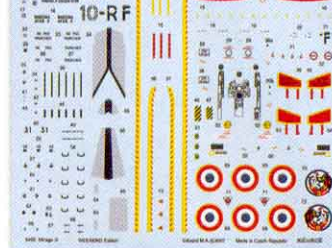
Decal Options: 1

Panel Lines: Recessed Status: Revised Tooling

Type: Injection Moulded plastic

Parts: Plastic 151, Clear 10 Manufacturer: Eduard

UK Importer: Hannants/LSA



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Westland PV-1
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G-ACBR

1:72



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RAF BE2C URO-426 £18.99

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PFALZ D.III URO-613 £34.99

1:32

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SAMI's reviews section has evolved into a well-respected encyclopaedia of kit-building. While space precludes covering every kit received with a full review, we do aim to give a representative cross-section of the entire hobby, including samples of the numerous reboxings and reissues that make up such a large part of the market.

We are always looking for new reviewers, so anyone interested in becoming part of the team is invited to contact Gary Hatcher at the editorial address.

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Sopwith Camel 'Black Mike'

EDUARD 1/48

BY DAVID RYLEY



Eduard's Camel has been around in various guises for three or four years now. This latest, limited-edition boxing offers little we haven't already seen except for the new decal sheet. This provides markings for the rather spectacular post-war mount of 28 Squadron's Canadian ace Lt Clifford Mackay 'Black Mike' McEwen (mis-



spelt 'McEwen' by Eduard!), as photographed in Florence some time in early 1919.

Construction

I'm sure previous reviewers have already covered the construction of Eduard's Camel in some detail, so I'll be brief here. This is a fairly complex little kit, which requires patience and close attention to the instruction sheet. However, most of the parts fit beautifully and if you put your trust in Eduard's instructions you can't go too far wrong. This shows up most markedly when attempting that most daunting biplane task, fitting the top wing and struts. This can go horribly awry when building a kit whose parts are not engineered precisely, but with a carefully made jig and a good deal of dry fitting the Camel's wing sits just fine. On the down side, although the manufacturer has taken the trouble to provide three sets of separated ailerons, none of them fit too well. The



inclusion of a piece of 'decking' intended to display the finished model on seems like something of an afterthought.

The etched fret is chock full of tiny parts with many options for the various pipes and chutes, plates and hatches. Many of the parts are not appropriate for Black Mike's Camel and should be consigned to the spares box. Many more were left off the review model due to tired eyes and a shaky hand, but as usual with modern Eduard releases the etched parts are an optional rather than an essential part of the build. Can the 'Weekend Edition' be long in coming?

represented on the box art, could seem a little daunting to some. These are supplied in five pieces, and the main problem is obviously going to be conforming the decals around the sharp, curved angle of the front cowling. This is dealt with on the sheet by small breaks in the bottom edge of the decal that help reduce wrinkling. Eduard also provide plenty of spares to give the poor modeller a second chance with the trickier parts, and allow for patching up mistakes. Another nice touch is the provision of decal streamers to be attached to the struts, although Eduard offer no clue as to how this is to be accomplished.

When so much thought and care has been lavished on the design of the difficult cowling pieces, it's a shame that the other striking marking, the large 'R' on each side of the fuselage, has been executed less than accurately. Reference to the well published photograph of this aircraft show the 'R' to be considerably more stylised than on Eduard's decal sheet.

Conclusion

Eduard's Camel is well up to their current high standards, being in the main accurate and well engineered. This post-war colour scheme is an interesting and attractive choice, but I can't help thinking that a dedicated etched set and a little more attention to references could have made it a truly Special Edition.

SAMI

Colour Options

This special edition provides just one, admittedly eye-catching, colour scheme. This should be where the kit scores highly, and indeed the decals work very well in the main. The red and white checkerboard plumage, well



F-14A Bombscat

ACADEMY 1/48

BY STEFAN GOOD

The kit contains eight light grey sprues plus the clear parts all in separate bags. On looking at the parts before construction they all have nice crisp engraved panel lines except for some of the nose section, which isn't as well defined as the rest of the model.

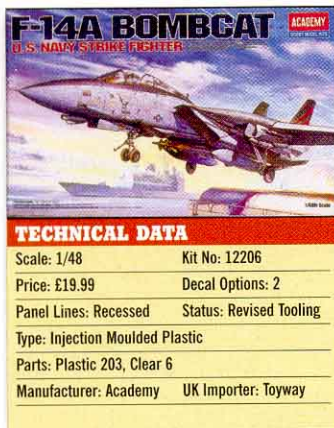
Construction

The instructions take you through sixteen fairly simple construction illustrations, starting with the well detailed cockpit which only requires care with the painting of the raised detail to make a very presentable start to the build.

The nosewheel bay fits to the bottom of the cockpit tub, however its fit to the lower fuselage nose section is quite vague, as is the cockpit's fit to the upper fuselage.

From there on in the construction of the main fuselage and wings is quite straightforward with no problems and only small amounts of filler required around the air intakes and trunking sections.

When the upper and lower front fuselage parts are joined it is difficult to



get the parts to mate smoothly together so a fair amount of filler and sanding was required to get a smooth finish with the subsequent loss of some of the engraved panel lines.

The undercarriage is very well moulded and you certainly get a very large choice of ordnance to complete your model with.

Colour options

There are two colour options, both for aircraft of VF-154 'Black Knights' during a



cruise in 2003 while on the way back from the Gulf. Both options are almost identical except for serial numbers, which seemed to be a wasted opportunity to me.

On trying to apply the decals I found them awkward to work with. They didn't slide well on the model and were also quite brittle and broke up easily. One other problem, which I've never had before, was that the painting instructions quoted the colour of the upper surface of the aircraft as FS 35237, which I matched to the Hannatts Xtracrix range. However, when I tried applying the decals the colour was so similar I couldn't actually see them except in very strong natural sunlight - they were completely invisible under artificial light!

The choice of decals for the ordnance was very large if awkward to work with.

Conclusion

I felt this was certainly not a shake and bake model, which isn't always a bad thing in that it made me work a bit harder than I have in some of my recent modelling. However I did feel that the kit was let down by just a few poorly moulded parts, and that if they had been up to the standard of the rest of the kit it would have made a big difference in my overall opinion of the model. That said I can recommend it to anyone if you take care in the aforementioned areas.

SAMI

Bristol 118

CHOROSZY 1/72

BY DAVE HOOPER

The Bristol 118 was a two-seater high performance multi-role aircraft that never made it past the prototype stage. Two prototypes were built and one would perhaps surmise that this is the second prototype from the 'second version' title on the box of Choroszy's latest kit. However, this is not the case as K2873 was a later registration and repaint of the original prototype, 7561 (originally marked R-3). The quality of resin pieces is as good, if not better than any I have previously seen. The clean detail and fine panel lines are typical of Choroszy's output while the lack of visible air trappings is commendable. A set of scale plans is included and these are likely to be more useful than the constructional diagrams which in some cases are shown as mirror images suggesting that parts be fitted to the wrong fuselage half. Colour descriptions are basic but adequate.



Construction

I found this kit to be reasonably simple to put together with all the major parts fitting extremely well. The engine mount assembly proved to be the biggest challenge as the crown shaped mounts were not only extremely delicate but failed to fit perfectly. These parts were



modified by carefully slicing thin slivers away with a scalpel until a good fit was achieved. Of particular note is the impressive sidewall detail on the wafer-thin fuselage parts. Another very nice touch is that the ailerons, elevators and rudder are all separate pieces allowing for some simple creative positioning. I did replace the interplane struts, but the use of the original pieces as successful templates would suggest that they fit reasonably well.

Colour options

As already mentioned this kit depicts 7561 with its later markings (circa 1932), however the decal sheet is obviously the same as provided with Choroszy's previous Bristol 118 kit as markings for R-3 are also included. As one has come to expect from Choroszy Modelbud the decals are very well printed, with a good colour register,

although I fancy that they are a little thicker and stronger than I have previously experienced from this manufacturer.

Conclusion

Wow! This really is a nice little kit. Ok, it's not quite a 'shake the box and out it pops' type of product but show me a resin kit that is. Saying that, it didn't require too much effort to put together, with the added bonus that there was very little rigging to tackle. Perfect if you've just completed a rigging nightmare and never want to see the stuff ever again. Needless to say, I was very pleased with the finished item which looks very elegant in its silver doped finish. Three cheers for Choroszy Modelbud for tackling another interesting and obscure interwar subject with such finesse.

SAMI



Bristol Brabazon

F-RSIN 1/144
BY ANDY McCABE

First impressions on opening the box are of a very well produced kit with 21 Resin and 11 White Metal parts, 1 decal sheet and 1 A4 colour page with an exploded diagram of the kit and a colour side view of the aircraft. The fuselage parts are very nicely moulded with fine panel lines and aluminium skin panels nicely depicted; a few resin pour marks around the tail are evident but these can be easily addressed.

The fuselage is split into three sections; the forward section is a solid resin casting which will eliminate the need to add nose weight, the centre section is hollow cast and incorporates the inner wings, engine intakes and engine cowl, while the tail section is again part hollow cast and has the tail and tailplanes moulded on.

There are positive locations cast into the three fuselage sections that allow for alignment and thus ensure that each section is correctly aligned with the centre section.

The white metal undercarriage parts are crudely cast and will need cleaning up to use, but the propellers, also metal, are excellent and will need only a quick clean up before use.

The decal sheet is nicely printed with good colour registration and clarity.

Construction

Construction begins by separating the resin parts from the pouring plugs and then cleaning them up. Very few pinholes were noticed but the surfaces of the wings, tail and tailplanes are very rough and will need sanding with wet and dry paper to achieve a smooth finish. The actual build begins by assembling the three fuselage sections together, and this is where the first big problem shows itself. There is a big difference in the diameter of the rear fuselage section to the centre fuselage section; the bigger step is towards the bottom of the centre rear section with a smaller gap around the top. To eliminate the smaller gap around the top the positive location plug and slot were modified so that the aft fuselage section could be pushed upwards, which made the gap at the bottom larger as a result, but I decided to keep as much panel detail as possible on the upper surfaces and dealt with the filling and



rescribing on the underside of the fuselage.

These two sections were assembled and filled with car body filler before the forward fuselage section was fitted into place. There was a slight variation between the centre and forward fuselage parts that again needed filler but this was nowhere near as bad as the rear section.

The two outer wing sections have rectangular plugs that fit into rectangular sockets on the inner wing halves, which needed modifying to allow the leading



edges of the wings to line up with those on the inner wing halves, but a decent fit was achieved with comparatively little trouble.

The white metal undercarriage legs would need a lot of work to clean up, and the main undercarriage legs actually bore little resemblance to those fitted on the actual aircraft, and no matter how much work was done to them I did not think that they were good enough so I



discarded them and scratchbuilt new ones. The kit-supplied parts have small location pins where the wheels were to be located, but bearing in mind the weight of this model (it is virtually solid resin) I felt this would be insufficient, so the scratch built ones would have brass rod axles.

The forward undercarriage leg was acceptable albeit with modification and some scratchbuilt additions. Replacement undercarriage legs were made from different thickness styrene tube and rod with the axles made from brass rod.

Supplied with the kit are eight resin main wheels. The Mk I Brabazon was fitted with only four main wheels and tyres. These were huge, and appeared to be at least twice the thickness of a single wheel and tyre, the eight kit parts were glued together and the gap between them filled and sanded flush.

Had the Brabazon reached a subsequent version it would have had eight wheels and tyres so the kit is not entirely inaccurate.

Other items from the kit that needed to be scratchbuilt were the 16 exhaust outlets on the underside of the wings, the nose probe, aileron and rudder actuators on the wing, tail and tailplanes, and various antennae on the fuselage.

Colour Options

Only one option is provided as the Brabazon never got past the prototype stage and only one complete airframe was built and flown. The aircraft was finished in natural metal all over, although you do have the option of applying black bands and upside down 'T' decals, which were applied to the sole prototype during flight testing, and without which the aircraft looks somewhat bare.

It would be nice to see this model with the BOAC livery, as I should imagine it would be quite impressive - perhaps F-RSIN will do this as an option sheet at some time?

Conclusion

All kits of this nature require a lot more work than mainstream injection moulded kits and you have to be prepared for this and accept it. This kit does need a lot of extra work to get it to the finished stage, and though you could make it virtually

straight from the box you would still have to correct the mis-match in the fuselage diameters.

The items that were scratch built and rebuilt were purely a matter of personal taste, and you would still have a very impressive model of the Bristol Brabazon without them, but I am a glutton for punishment.

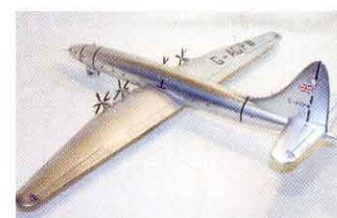
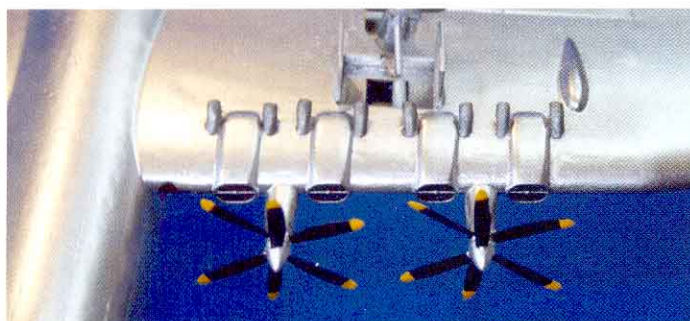
Despite all of the problems experienced I wanted to do the model justice and get it right as it does deserve the extra work and there are very few kits, even mainstream ones, that do not benefit from modification by modellers at some point. Research is vital before commencing on this kit as reference photo's will show what extra bits need to be made and where they go. A good source for these is at www.aviationarchive.org.uk and I am indebted to John Battersby at the Bristol Aero Collection for taking the time to trawl through their archives for images of the main undercarriage.

The kit is expensive and at £67.00 (80 Euros) you may ponder as to whether it is worth paying this much money for a kit that needs a fair amount of work, but trust me it is, and if you get the chance to see one up close you will see for yourself.

As far as I know this is the only 1/144 scale model kit of the Bristol Brabazon on the market and is a timely reminder of how adventurous and bold the British Aviation Industry used to be. The Brabazon legacy led to a home for the British side of the Aerospatiale/BAC Concorde project, as the hangars that were built to build the Brabazon are the same as those used to build the British Concorde.

This is an important and impressive kit for any Civil Airliner modeller. Now, how about a Saunders Roe Princess in 1/144 from F-RSIN?

SAMI



F6F-3 Hellcat

HOBBY BOSS

1/72

BY CLIVE DUCKWORTH

Looking at the box ends I see 'easy assembly' written, and wondered if this was shorthand for 'snap together,' with few parts and little detail. Well let's have a look inside the sturdy box and see! Here there are 24 injection parts in light grey plastic, 22 of them on 2 sprues plus the complete fuselage, and main wings neatly secured in a moulded tray. The 3 clear parts are kept scratch-free in their own compartment. There is a glossy instruction leaflet, including the 5 part diagrammatic build instructions and two colour options on the back page. The decals have their own poly bag.

The one-piece fuselage has excellent detail with scale raised rivet lines and even a radio aerial on the tail fin (still intact owing to the moulded tray). Similarly, the one-piece wing section has impressive detail including the ailerons, flaps, wheel wells and 6 machine guns.

Construction

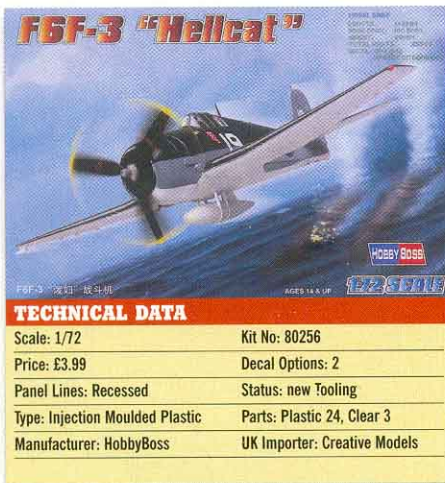
The first task is to attach the seat to the floor, paint the entire interior matt cockpit green, and fit the floor up into the fuselage without snapping off the joystick. Next come the wing and the tailplanes. The fit is excellent with no filler needed due to a number of locating pins and good manufacturing. The fit on the tailplanes is particularly impressive as the dihedral is very precisely held. In almost no time at all a nearly complete model emerges from the modest collection of parts.

On with the build and the prop to engine to fuselage fit is again very good. The prop has the usual small boss to be fixed from the inside the engine cowling to hold the prop on but this isn't shown on the instruction sheet. Undercarriage and fuel tank all fit perfectly. Cockpit side windows slide in first and then the main canopy is fixed. This latter is exceptionally clear and a perfect fit.

Looking at the unpainted model for any cleaning up of joints that might be required, I found a little scraping necessary behind the wing section where it joins the top part of the fuselage... no filler anywhere! My only additions to the kit were representations of a seat harness with Tamiya tape, and a decal for the somewhat large and visible instrument panel.

Colour Options

Markings include a 1942 scheme described as 'medium grey' in the Gunze Sangyo colour key, with white undersides, or a 1944 version described as 'deck blue' and 'blue/grey' with white undersides. I opted for the 1944 USS Intrepid scheme. The decals are excellent but the instructions are a little confusing. The



TECHNICAL DATA

Scale: 1/72	Kit No: 80256
Price: £3.99	Decal Options: 2
Panel Lines: Recessed	Status: new Tooling
Type: Injection Moulded Plastic	Parts: Plastic 24, Clear 3
Manufacturer: HobbyBoss	UK Importer: Creative Models



colour diagram shows the star and bars on the top and bottom of the port wing and the numbering mostly doesn't cross match with the decal sheet, although this is not a major problem if you have some prior knowledge of the wing insignia arrangements. With the 1942 scheme however I understand the colour is more likely to be blue/grey not medium grey and I think the white star/blue circle should be above and below both wings for the date. Only 2 decals are provided for the wings in the kit for that version.

Conclusion

Overall a well-engineered kit, which for the price provides an excellent quick-build finished model. The instructions are fine but the part numbers for both the plastic components and decals are mismatched or duplicated. If you want a quick finished model and prefer the painting and decal side then this kit is great with its few parts and excellent fit. The box advice says 'age 14 and up'. I'd have thought this kit easy for much younger modellers, and would not hesitate to recommend it be offered to such.

SAMI



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Grumman F4F-4/Martlet Mk.5

REVELL **1/32**
BY TIM LARGE

Dating from 1969, this old chestnut comes on five sprues, four of which are in a mid grey plastic, holding sixty parts. The exterior detail is all raised, but it's well done, being subtle and acceptability accurate – 'state of the art' for its time! The clear sprue contains four items, which again are well moulded, and there is surprising little flash or blemishes on any of the plastic, considering its age. A large decal sheet provides markings for two aircraft, one British and one American. The instructions are a simplified version of Revell's usual affair, in an A4 format of seven pages, with eighteen sections for the build and two for the painting and decal placement.

Construction

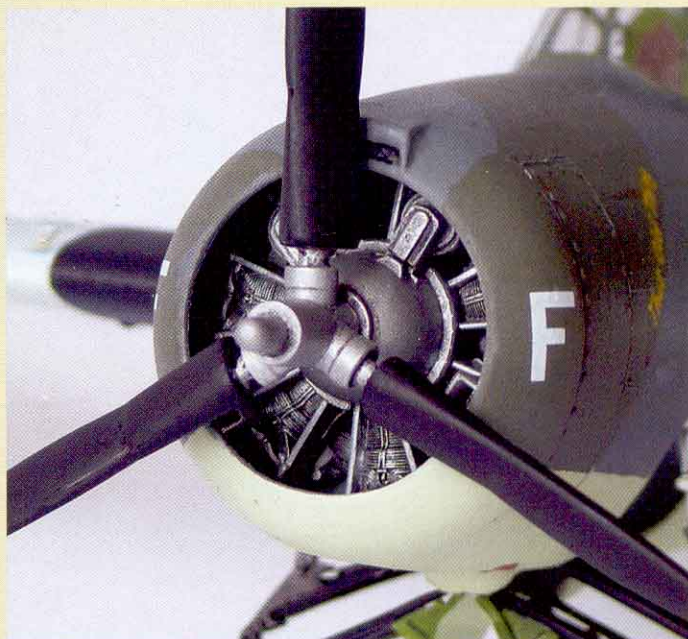
This is one of Revell's better large-scale kits. You get a nicely detailed four-part



TECHNICAL DATA

Scale: 1/32 Kit No: 04784
Price: £14.99 Decal Options: 2
Panel Lines: Raised Status: Reissue
Type: Injection Moulded Plastic
Parts: Plastic 57, Clear 4
Manufacturer: Revell

engine, a simple, yet accurate cockpit and the complicated undercarriage assembly is well reproduced. The overall fit of the parts (once cleaned up), makes the model a pleasure to build. The wings are designed to fold in a manner as near as possible as the real aircraft, so make



Il-2 Shturmovik

ITALERI **1/48**
BY MIKE KING

Italeri have reboxed the late-war two-seater version of the late 1990s Accurate Miniatures 1/48 Shturmovik, with five decal options, for roughly 2/3 the price of the original. There are seven sprues with some 110 medium-grey parts, and one with 9 transparencies, decals and a standard-format, 10-page, Italeri instruction sheet.

The grey plastic moulds are beginning to show their age. They have significantly more flash, visible sink marks and mould-separation lines than the first pressings. The transparencies, however, are still impressively thin and clear.



Construction

Construction starts with the very well detailed cockpit, supplying much more than you'll be able to see on the completed model. I replaced the seat belt decals with wine-bottle foil items. Had I not been building this model strictly OOB, I would also have replaced the gunner's seat-strap with something a bit less clunky.

With careful handling, the fuselage halves and nose halves join together almost perfectly. However, installing the wing cannon barrels and the rudder



TECHNICAL DATA

Scale: 1/48 Kit No: 2657
Price: £17.99 Decal Options: 4
Panel Lines: Recessed Status: Reissue
Type: Injection Moulded Plastic
Parts: Plastic 106, Clear 10 Manufacturer: Italeri
UK Importer: The Hobby Company

mass balance at this stage, as the instructions suggest, is courting disaster, so I omitted them until final assembly

The wing to the fuselage fit is very tight, and some filling and sanding was necessary to ensure a clean join. By contrast, the tailplanes fitted to the fuselage perfectly, with no need to clean up the join at all.

It is important not to cement the propeller in place before you install the engine exhausts, since these fit in place from the inside, and the plastic around the front of the nose is very thin. Guess who found this out the hard way?

Fit of detail parts is generally uneventful, though the exact placement of the rear machine gun mount proved tricky. Stores include 250kg. bombs, unguided rockets and 37mm anti-tank cannons in gondolas. I left these last off because the subject of my build didn't carry them.



There are optional bulged and unbulged tyres. I used the latter, filing a small flat on both of them. The main wheel hubs are moulded separately for easier painting, but the main undercarriage legs and supports need careful test fitting.

Colour Options

Five finish options are included, four Soviet, two in two-tone earth/ topside green camouflage and two more in three-tone earth/ topside green/ very dark grey. The post-war Polish machine is plain dark green/ light blue. I chose

one of the two-tone schemes, with the Patriotic Slogan 'For the Motherland!' on the side of the fuselage.

Colour call-outs are given for the nearest shade in the Model Master range to that actually used.

Decals are up to Italeri's usual high standard, thin, well printed and in-register. However, there are only enough correctly sized white-outlined red stars for the underwing markings of one of the Soviet aircraft. For the rest, you have to trim a pair of the red-outlined stars. Not good. Technically, though, the decals were superb – no silvering at all, even over sharply-curved surfaces.

Conclusion

Accurate Miniatures's Il-2 is the definitive 1/48 Shturmovik. The Italeri reboxing has the advantage of five decal options instead of one, and is much cheaper. If you can cope with some tightly fitting major components, and the lack of suitable underwing Red Stars, it comes thoroughly recommended.

SAMI





sure the hinge is well glued into place on both the outer and inner parts, or you will end up with droopy wings, which won't stay in place.

The only filler I used in the build was around the clear windscreen, where it fitted to the fuselage.

Colour Options

The British aircraft is for a Martlet V 'That Old Thing' of 832Sq RNAS Eglinton, June 1944, this is in the normal FAA finish of Extra Dark Sea

Grey / Dark Slate Grey upper and Sky lower, with invasion stripes (supplied as decals). The American version is for a F4F-4 of VC-12 USS *Coral Sea* Autumn 1943, this comes in the North Atlantic colour scheme of Non Specular Dark Gull Grey upper surface, with Insignia white fuselage sides and undersurfaces. The decal sheet is nicely printed and in perfect register, as you would expect from Revell, and as usual from this manufacture, comprehensive. You are treated to a full set of stencilling as well as the relevant national markings and the individual squadron serials and codes.

With some trepidation I went for the FAA version and the large invasion stripes decals. I needn't have worried, as they, along with the rest of the decals used, adhered to the surfaces with hardly a whimper. I did use Micro Sol just to help matters along. Two problems I encountered were bleed-through on all the white parts of the decals, and the misprinting of the side stripes, which would not match up.

Conclusions

Taken on its own this is a nice, simple, well detailed and accurate kit. There is a lot of scope, with an Eduard set available, as well as a large number of decal sets, to enter the world of super detailing on a first-time big kit and test your skills.

But of course the newly tooled Trumpeter kit is now out, so one point to ponder is why Revell released this kit, when they do have other moulds in this scale of aircraft not made by any one else, such as the Hurricane, Typhoon or the Mosquito? Perhaps they are just waiting in the wings?

SAMI



Dewoitine D.1bis

CHOROSZY 1/72
BY CHRIS BUSBRIDGE

Yet another high quality product from the prolific Choroszy Modelbud. Tucked away inside the familiar white and blue lid-opening box are the usual cream resin parts for the D.1bis - a French parasol design first built in 1919. Components are neatly packaged in sealed bags, and inspection of the pieces reveals a good level of detail, high quality casting and very little evidence of air bubbles. Some preparation is necessary, such as the leading edges of the wings and where parts have been removed from their casting blocks.

Construction

As the aircraft had an overall silver scheme, I assembled as much of the model as possible beforehand. The cockpit is comprehensively detailed and fitted neatly inside the fuselage, although the instruction sheet gives no



clue as to what the internal colour should be. After closing the fuselage, only a smear of Mr Surfacer was needed to hide the join line, and all that internal detail was hard to see. The wing, struts and propeller were kept as separate parts until the painting was completed.



Colour Options

The silver scheme was achieved by spraying Alclad Aluminium over a polished grey primer. It did accentuate the panel work on the fuselage a little, suggesting they had been engraved a bit too deeply. After fixing the wings in place, the wings struts did not seem to be long enough, either that or the guide marks on the wing were in the wrong place. A wash was used to bring out the detail on what I guess are the radiator units attached to the undercarriage.

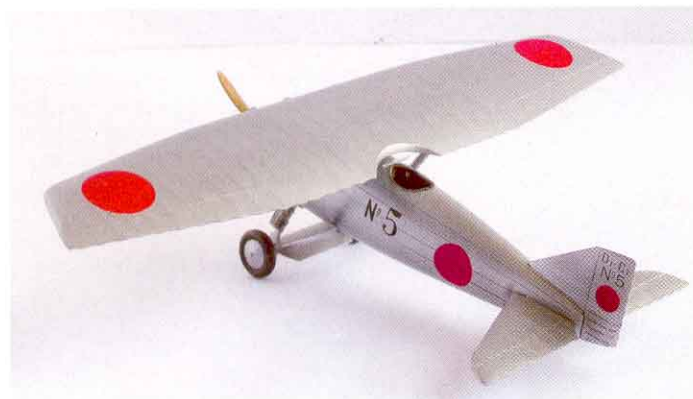
The decals had a very thin varnish, which made the smaller items rather delicate to handle, but at least it minimised the risk of silvering.

Conclusion

This is a very simple model to construct and can be recommended to all. It could also make a suitable conversion project for anyone wanting

to build an Ansaldo AC2 or a license-built D.1ter, both of which were flown by the Italian Air Force. As well as France and Italy, other nations to utilise this aircraft were Switzerland and Belgium. According to the historical notes, the markings on this model represent an aircraft tested by Japan sometime in the twenties.

SAMI



Mil Mi-2 'Hoplite'

OKit **1/72**

BY DAVID HOLMAN

Mil Mi-2 'Hoplite' Warsaw Pact & Air Force

Mil Mi-2 'Hoplite' Post Cold War and Exotic Schemes

Every now and then you wait a long time for a decent kit to appear of a not so well covered subject and just like the 73 bus, two come along at once! This is just such an occasion with the release of these very welcome multi-media kits from the Czech Republic, although there has also been some debate about these being reissues of the old InTech kits from Poland (see SAMI May 2001).

Inside the sturdy boxes featuring photos of actual aircraft, the grey injection moulded parts with crisp recessed detail are nicely secured, with the resin and photo etched parts bagged separately. The mouldings for both kits are identical with the addition of an extra frame featuring the rocket pods and missiles for the Warsaw Pact/Air Force version. Some of the resin update parts need a bit of preparation to remove the casting blocks and clean off the residue.

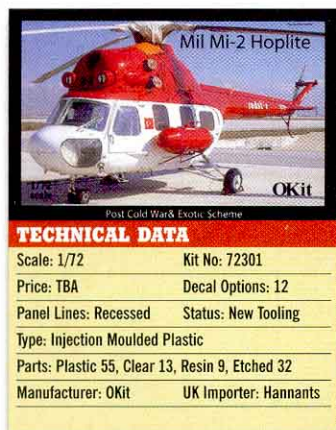
Construction

Construction begins with the placing of the windows for the cabin and then it is on to the cockpit and interior. The combination of resin and photo-etched accessories provides an accurate enhancement to the kit parts in this area. The fuselage halves on both kits fitted without problems, as also did the engine housings. Don't forget to add some weight to prevent tail-sitting, as there is no indication of this in the instructions. I found out the hard way and had to put some lead shot into the external tanks in order to countermand this error.

The only serious filler requirement was along the top of the fuselage and

on the underside of the tail booms. I also found the need to use a little around the fit of the main transparency. The external attachments such as the undercarriage struts fit into their respective recesses with the resin replacement wheels being more accurate in detail than their kit counterparts. The etched parts are a little fiddly but really make a difference to the appearance of the model.

The two-piece rotor heads are fairly straightforward with the rotors fitting into small locating plugs, and once assembled, they allow for free rotation.



The fitting of the exterior mountings for the fuel tanks are clearly marked on the diagram and are in correct alignment, with the fuel tanks fitting onto the etched supports at the correct angle to the fuselage. The side-mounted cannon, rocket pods and missiles to finish the Warsaw Pact/Air Force version as an armed Mi-2URN are included and are accurate in scale.

Colour Options

One of the most welcome features of these kits has to be the amount of different finishing options available to choose from. The decal sheet for the Warsaw Pact & Air Force' version has markings for 12 aircraft from the USSR, Russia, Poland, East Germany, Bulgaria, Czechoslovakia, Slovakia and the Czech Republic. The 'Post Cold War and Exotic Scheme', also has decals for 12 aircraft including examples from Mexico, Germany (Police), Turkey Redstar Aviation (as the box photo) United Arab Emirates, Poland, Belarus and Greece.

I decided to finish my Warsaw Pact/Air Force version as an unarmed Czech air force machine in a three-tone camouflage scheme and my Post Cold War/Exotic Scheme model as the box



photo of a red and white air ambulance operated by Redstar Aviation of Turkey. The decal sheets are printed well with no discolouration evident on my examples and I found the location diagrams for both kits clear and easy to follow. However, the application of the decals was a different story altogether, as I soon discovered that they require only a dip in water or they will start to break up. I lost the main Redstar tail boom emblem on the starboard side for my Turkish air ambulance example by leaving it in the water slightly too long.

Conclusion

According to my downloaded PZL publicity brochure reference, the length is 11.9m and the rotor diameter 14.6 m, and the two models measure out near enough accurate to these dimensions in scale. These two kits have been a pleasure to build; sometimes very challenging and with the inclusion of the multi-media update sets they allow plenty of activity for the more experienced modeller (and his *Etch-Mate*). Now all I need is another twenty-two of these kits to replicate the set up of all twenty-four finishing options seen at Nuremberg this year.

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Republic P-47D Thunderbolt

HOBBYBOSS 1/72
BY CHRIS HALL

This is another of HobbyBoss' easy-build kits, aimed squarely at the younger or casual modeller. Attractively-presented in a black plastic tray, holding the parts in place and protecting them without needing poly bags, there are two grey and one clear sprues, and a number of individually-moulded items, including the single-piece, slide-moulded fuselage and wings/ lower fuselage, totalling 39 parts. There are more flash and seam lines than you might hope for from a new mould and several deep ejector-pin and sink marks.

The instructions are presented in four stages. The decals give two finish options.

Construction

Fit of parts is tight and good, but not



TECHNICAL DATA

Scale: 1/72	Kit No: 80257
Price: £3.99	Decal Options: 2
Panel Lines: Recessed	Status: new Tooling
Type: Injection Moulded Plastic	
Parts: Plastic 37, Clear 2	
Manufacturer: HobbyBoss	
UK Importer: Creative Models	

perfect. The major seams need fairly extensive cleaning up and some filling.



Assembly, unusually, doesn't start with the cockpit, because it's moulded integrally with the fuselage. There's a seat and control column, and a separately moulded headrest. I added a control panel from the spares box, and Tamiya tape seat belts.

The engine is made up of two parts and there is a choice of cuffed and uncuffed propellers. The cowling lacks sufficiently defined cooling flaps, so I

opened them up and deepened the panel lines in this area.

Undercarriage assembly was uneventful. Large locating lugs and pins ensured proper alignment. The huge locating lugs for the underwing pylons and stores look chunky but don't significantly compromise scale accuracy.

Assembly is completed with the cockpit canopy, which is thick and distorts what

Super Constellation

REVELL 1/144
BY ANGUS McDONALD

Some years ago, my son and I were at the Middle Wallop airshow. It was drizzling. We took shelter under the port wing of a Super Constellation to have our lunch. After a while the crew arrived and prepared the Connie for take off, so we simply strolled over and took cover under a Viscount to watch the Connie do her flying circuit. She was quite a sight, bombing down the runway at about 50 feet, then screaming upward in a port turn. Such nostalgia wouldn't be allowed today by the Health and Safety killjoys, hence, with great anticipation, I started building Revell's all new Super Constellation.

It's a civilian subject so the plastic must be white. Not too bad to work or scribe. For a modern kit there was a surprising amount of small-scale flash on quite a few parts, some of which were also quite roughly finished, with signs of pitting. Ejector pin and sink marks were, happily, absent. The engraved panel lines seemed a little on the deep side, otherwise parts were sharply moulded with good detail.

You have a choice of a long-nose or short-nose airframe, with or without wing tip tanks.

The large decal sheet is the size of the box and contains two colourful schemes (TWA and Lufthansa) for about five different aircraft.

Construction

Use the excellent instruction 'booklet' as a guide, leaving all dangle and easily broken bits for as long as possible. I didn't use the transparencies for the passenger windows. Kristal Klear was employed when all painting and decaling was done.

Commence with the cockpit, which comes with three seats, control sticks and



TECHNICAL DATA

Scale: 1/144	Kit No: 04252
Price: £12.99	Decal Options: 3
Panel Lines: Recessed	Status: New Tooling
Type: Injection Moulded Plastic	
Parts: Plastic 93, Clear 7	Manufacturer: Revell

a decal for the instrument panel. Absolutely none of this can be seen through the cockpit transparency, so it's best left out and the area crammed with weight. I filled the area behind the last seat with lead, and just avoided a tail sitter!

I elected to do the short-nose version of the Super Connie. The alternative part for this is moulded in clear plastic, probably because of the light in the nose. Sadly both this, and the cockpit transparency, did not fit the fuselage well, with quite a large step necessitating filler to blend them in.

Fit of the fuselage halves is excellent but the central fin is appallingly thick. Its leading and trailing edges aren't sharp, and the decals destined for it will not join up properly. By contrast the outer fins are well moulded with sharp trailing and leading edges, and fit the tailplane in an effective and ingenious manner.

Two upper wing halves join onto a full span lower wing. There is an interesting mechanism to engage and lock onto the fuselage. Unfortunately, the wing trailing edges are disappointingly thick, and do not match with the fuselage wing root, so I joined the upper wing halves to the fuselage to get a reasonable fit, and some filler was required to rectify the resultant step.

Engines are beautifully moulded with lovely fins on the cylinders, all but hidden by the cowls and a huge boss that fits on the front of each. The carburettor intakes were not a brilliant fit. The cowls did not match well with the nacelles moulded on the wings: there was a bit of a step, but a bit of judicious scraping fixed this.

Undercarriage parts are well detailed and fit nicely, though the doors are, as usual, quite thick. None more apparent than the nose door where one part (in the real aircraft) slides forwards and sits flush with the fuselage. Although I used it, I would recommend its replacement with something much thinner.

My major gripe is with the increasing modern trend of providing various blade aerals and pitot tubes that butt join the fuselage, and sometimes the wing. All the aerals and tubes that fit on the underside of the Connie's airframe are of this type. It was a real pain to get them to glue into place, align them and attach some rigging line between four of the lower aerals. I don't mind drilling holes to ensure a sturdy and precise fit.

Colour Options

The decals are wonderful, and of reasonable register. The ones running down

the side of the aircraft align well with the passenger windows, however, when doing the Lufthansa schemes, ensure that the part of the decal in front of the last passenger window slopes downwards so that it aligns itself with the sides of the wheel wells. This will ensure that it matches up with the decal used for the antiglare panel in front of the cockpit. You will also have a devil of a time mixing paints to cover the tip of the nose, which the side decals do not cover. I used Citadel's Enchanted Blue with a dash of Regal Blue.

Conclusion

In a word: disappointment. Revell have been setting such high standards of late and this kit was so eagerly anticipated. The far too thick central fin, the thickish wing trailing edges, the poor fit of the clear parts and engine cowls to nacelles, and the need to fiddle with paints to match the Lufthansa decals, means that much work is needed to turn out a fine model.

Recommended for medium skilled modellers prepared to put in some old fashioned modelling skills or beginners who aren't too fussed about the fit.

SAMI



you can see through it – so maybe the lack of detail doesn't matter so much anyway.

As well as the propellers, you get a choice of 105-gallon drop tanks or 500lb bombs for underwing stores, and a 150-gallon flat tank below the fuselage.

Colour Options

Two options are provided. 'Rabbit' of 527FS, 86FG, is in overall natural metal, with red striping on the tail surfaces (provided as decals) and red stripes on the cowling and red spinner tip, while 'Big Ass Bird II' of 531FS, 406FG, has red/blue/yellow tail stripes (provided as decals) and a red, scalloped nose, black bands on the tailplanes, and under-fuselage invasion stripes, all of which have to be painted on.

I painted the cockpit interior in Humbrol 149, the undercarriage bays in Zinc Chromate, and the wheel hubs and legs painted aluminium (not natural metal). The

engine and cowl interior was painted matt black to hide the lack of detail.

For the natural metal finish, I used Humbrol Metalcote Polished Aluminium, with Bare-Metal Foil on some panels. After decaling, a dusted coat of Humbrol Mattcote reduced the level of shine.

Decals were thin, and went on with few problems. However, the tailfin stripe decals needed to be cut into three pieces to fit properly, and the fuselage markings needed careful trimming to fit over the intercooler exhausts.

Conclusion

According to *Wikipedia*, the P-47D was 11.00m long with a span of 12.44m. In 1/72, this translates to 152.8mm and 172.8mm respectively. I measure the kit at 155mm long and 174mm wide. Close enough. It also exhibits a sufficient degree of Thunderboltiness, which is what really matters.

However, when you get up close you

can see where this kit falls down – detail. There are a number of significant problem areas, including:

- Total lack of any engine detail
- No turbocharger intake or oil cooler detail
- Intercooler exhaust vents moulded solid
- No fixed trim tab on the starboard aileron
- Limited and inaccurate u/c well detail
- Store attachment points crude and inaccurate

- Wheel hubs inaccurate for the subjects of the decal sheet.

In fairness, the target market probably won't worry about any of this.

If you are looking for a quick build, this kit can be recommended. However, better-detailed, more accurate kits of the same subject, with much better decals, are available for the same price.

SAMI



B Mk 1 w/Tallboy 'Tirpitz Raid'

HASEGAWA 1/72
BY ANDY McCABE

Following on from Hasegawa's release of the Lancaster with the Bouncing Bomb this kit has the 12,000lb, 21ft Long (real weight and length not the kit length!) Tallboy. When dropped from 20,000 ft (6,100 m) it made an 80 ft (24 m) deep crater 100 ft (30 m) across and could go through 16 ft (4.88 m) of concrete.

Though the Tallboy was used on numerous other missions, the subject of this kit is the raid on the German Battleship *Tirpitz*.

Two squadrons were employed, these being 617 and 9. The first raid was launched from Yagodnik near Arkhangelsk in Russia on the 15th September 1944. This attempt failed to sink the ship but extensively damaged her and forced her to sail to Tromsø for repair, which put the ship within range of the Lancaster Squadrons flying out of Lossiemouth. Further raids failed to prove conclusive, then finally on 12th November 1944 the two squadrons attacked *Tirpitz* again and after three direct hits she capsized and sank.

The kit is supplied in a lift-off lid box with nice colour artwork on the lid. Inside are 13 sprues of injection moulded plastic, four of which are clear, and one more sprue of nylon washers that retain the propellers in place.



TECHNICAL DATA

Scale: 1/72	Kit No: 00832
Price: £29.99	Decal Options: 3
Panel Lines: Recessed	Status: Revised Tooling
Type: Injection Moulded Plastic	
Parts: Plastic 133, Clear 57	
Manufacturer: Hasegawa	
UK Importer: Creative Models	

The quality of the moulding is excellent, as is customary from Hasegawa, with nicely engraved panel lines and surface detail, and no flash was present on any part.

The instruction sheet is clear and concise, paint references given are for Gunze Sangyo and it is worth studying these instructions before starting, as there are numerous holes that need opening out before the fuselage halves are joined.

The decal sheet is crisply printed with nice colour density and registration.

Construction

Work starts with the interior, which is built around the floor/bomb bay roof. There is not a great amount of detail in the parts here and all instruments are provided as decals. Items include a pilot's seat, control column, instrument panel and navigator's station.

The wings are fitted to spars that are also fitted to the cockpit floor. The tailplane assemblies also fit onto a spar that is inserted into the fuselage before the two halves are joined, and this, along with



the wing spars give a strong positive location on both the wings and tailplanes.

The gun turrets are far superior in detail than the earlier Airfix kits, as they don't have the half armless gunner attached to them! There are aftermarket-detailing kits available for these, but they are acceptable straight from the box. The only addition I made to the standard kit was at the tail gunner's position, where there is a gap between the turret and the fuselage that is quite noticeable. I simply filled this with a piece of plasticard, and as the turret goes into the rear fuselage, a small section was cut from the new plastic panel to allow the turret to sit flush.

The construction posed no problems at all and no filler was needed throughout. The biggest job of all was masking the canopies, which took a few hours to complete – much easier to buy pre-cut masks I think.

The Tallboy itself is made up of six parts, and the vanes have the correct angle on them that allowed the bomb to spin on its descent to its target.

Colour Options

With very little cleaning up needed the model was sprayed with Humbrol Acrylic Grey Primer then with Humbrol 29 and 30 upper surfaces over Satin Black 85.

The decals are superb, no problems

were experienced during their application, and the colour density is such that the black surfaces do not show through the decal and discolour it.

There are three different decal schemes supplied, although the colour scheme is the same for all. Options cover both ED673/KCZ and DV385/KCA of 617 Squadron, and LM220/WSY of 9.

Conclusion

I can find no major flaw with this kit. The build was very straightforward, the decals superb and the detail is excellent, though questions have been raised with regards to the engraved panel lines where there were originally rivets. I like the engraved panel lines and prefer them, as they are easier to replace than raised rivets after sanding. There is some very finely moulded rivet detail on some of the parts, but not in areas that need sanding.

I will now have to invest in both the Bouncing Bomb and the Grand Slam versions.

This is by far the best 1/72nd scale Lancaster kit on the market in my opinion, and the end result is outstanding. Whilst the lack of detail in the cockpit is somewhat disappointing, especially from Hasegawa, it does not detract in any way from the quality of the product as a whole.

SAMI

Grumman F6F-5 Hellcat

HOBBY BOSS 1/72
BY BRIAN DERBYSHIRE

This kit is aimed squarely at the junior end of the market, but in some ways it's as good as any currently available 1/72 Hellcat. Surface detail is excellent, the propeller is the best yet, and the cockpit is a very acceptable basis for closed-canopy builders. It appears to be identical to HobbyBoss's earlier F6F-3, except for the transparencies and transfers.

HobbyBoss have tried hard to make the assembly foolproof, and almost succeeded – you can't assemble it wrongly. However, minor mould misalignment and flash mean that some fettling is still needed to make the parts fit perfectly.

The wing is, as usual for HobbyBoss, all in one piece. I haven't seen solid wings this thick since the Hawk kits of the '50s, but in this case there are no sink marks and the trailing edges are nicely sharp. The small parts are just as good. This kit could be built in an hour, painting (and canopy) excepted.



Instructions are clear and simple multi-stage exploded-view style, with colour profiles of the finished article. Perfect for kids. (However, they tell you to install the wrong radio mast of the two supplied).

Construction

Mostly dead easy. Being finicky, I chamfered the edges of the tailplane slots, and eased the pins and ridges on the belly, to improve the fit. I also removed the redundant outlets from the cowlings to suit the -5.

The only serious snag was the canopy, which is too narrow, too tall, and has too short a windscreen. So I filed and polished the windscreen to shape, and



replaced the canopy's sill tabs with strips cemented inside the fuselage. This spread the transparency to a perfect fit, but then the reshaped windscreen cracked, and had to be replaced by an Airfix spare.

Colour Options

Markings are included for two -5s in Sea Blue overall: one from Princeton's VF-27, with snarly face, and McCampbell's Minsi II from Essex. The transfers are basic – white stars-&-bars, numbers, and artwork, with no stencilling, serial numbers, or McCampbell's little CAG markings. This is fair enough for the target market. However, the transfer quality may be too high for beginners – they are very opaque, but also very thin, and therefore very flimsy. They snuggle down perfectly into the surface detail, but the Princeton 'eyes' are rather poor and I overlaid mine with Hasegawa's.

Conclusions

With exceptions, very good indeed. The fuselage is a touch long (doesn't show) and the canopy noticeably distorted. The drop tank is much too thin and lacks some support struts. The wheel wells are too shallow and have totally spurious detail. The cowlings are clearly for an F6F-3, and the simplified integral engine lacks magnetos. The wheels are basic. There aren't any cockpit rear windows on the kit, though they were still fitted on early -5s, including those for which markings are provided, and the pitot head is too far inboard.

I'm sure the target customers would say I was just nitpicking!

Not one for the superdetailer, but excellent for its purpose. Buy one for your nephew!

SAMI

Mirage IIIC

HOBBY BOSS 1/48
BY TIM LARGE

As soon as you hold the box there is a feel of quality and on opening, any doubts the modeller may have are dispelled in an instant. We are talking 21st century plastic moulding technology! All of the six sprues, five in a light grey plastic and one in clear, are crisply moulded, with fine recessed panel lines and raised detail where appropriate. There is some flash here and there and as seems the norm these days, a moulding line in the centre of the main canopy. The instructions are in a fold-out format, comprising fourteen sections to the build and a contents layout. A full colour painting and markings guide is included and a good-sized decal sheet rounds the package off.



Construction

The build is pretty straightforward, most items fitting without any trouble. There are some alternative parts, but no indication as to which version they relate to, so you will need to research your chosen aircraft. The instructions are also a bit vague as to the placement of some parts, so dry fitting is essential.

The wings are moulded with an alarming droop, although I managed to bend the worst of it out. On the other



hand the wheel wells are nice and deep and well detailed. The only other problem was the fit of the main canopy, as it is a little wider than the windscreen and a little too long. I showed it open which alleviates the above issues.

Hobby Boss supply more underwing stores than you can shake a stick at! There are so many I won't list them, but they should cover most people's needs.

Colour Options

Altogether there are four choices: two are French aircraft from EC 2/10 'Seine', one in Light Blue and the other in all over Silver. The third and fourth aircraft (French and South African) are in a camouflage finish of Sandy Brown/Dark

Earth upper surface with a Light Blue underside. The decals are first class, being well printed, with no bleed through. They adhered extremely well with no need for any solutions.

Conclusions

This was my first Hobby Boss kit and I have to say that despite the niggles, I was impressed by the overall quality of the package. Taking into account the price, you can't really go wrong here, so if you have the urge for a Mirage in 1/48th scale, I would recommend making this kit your first port of call.

SAMI



50
BATTLE OF BRITAIN MEMORIAL FLIGHT
1957-2007



BATTLE OF BRITAIN MEMORIAL FLIGHT

This Anniversary Collection contains the three classic BBMF aircraft; Supermarine Spitfire MkIIa, Hawker Hurricane MkIIc, and Avro Lancaster B MkI and an exclusive triple pronged stand enabling the aircraft to be displayed in their flying formation. The Battle of Britain Memorial Flight never fails to evoke emotion, from pride to remembrance, gratitude to admiration. The sight of this flight commemorates a nation's dogged determination and aspirations, and continues to inspire generation after generation. This year the Battle of Britain Memorial Flight marks an important milestone since its formation at Biggin Hill in 1957, its Golden Anniversary. 50 years on, its ever increasing popularity has meant that the crew now perform upwards of 700 individual aircraft appearances each year. Airfix are proud to be associated with the Battle of Britain Memorial Flight and to be able to offer the 2007 scheme in this new collection available in August.

1:72

www.airfix.com



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A Hornby Product

1:72

BATTLE OF BRITAIN MEMORIAL FLIGHT

Every month a bewildering selection of etched brass and resin accessories is released by the aftermarket manufacturers. The interest generated by these is enormous, and while far more are probably bought and sold than ever actually used, the fascination they hold for the modelling public is sufficient to warrant serious attention from magazines such as this. *Scale Aviation Modeller International* has a policy of featuring every item received, and this section has become a first point of call for many modellers on the lookout for those optional extras to superdetail their kits.

Manufacturers are invited to submit their products, no matter how limited the run, for consideration in these pages.

New Horizons

AMS RESIN

Most English modellers will know of the dramatic rise of Lewis Hamilton through the rankings in his first season of Formula One, well here is the resin equivalent. AMS Resin is Harold Offield, and his products are absolutely jaw dropping in their attention to detail. AMS decided for their first release to supply a detail set for the F/RF-84F but as there are a number of kits for this aircraft they have decided to release no fewer than nine different sets.

This makes things quite complicated but basically the F-84F has been produced in 1/48 scale by Monogram/Revell and the brand new Kinetic kit, which looks to be an improved version of the Monogram kit with recessed panel lines and a repositioned air brake. Heller have also produced an F-84F and the only 1/48 kit of the RF-84F. AMS Resin has noted all the differences between these various kits and each detail set has been engineered to fit the specific kit you are using with minimal work from the modeller.

The sample we have been sent is for the Kinetic kit but the basic contents are similar in all the other sets and as a bonus we have also been supplied with a test sample of one of their next

projects - a wheel well detail set for the F-80/T-33 family.

F-84F Detail Set European Version

Product No: 48006

Designed for Kinetic

Price: \$24.95

The photographs that accompany this review will probably not do the resin parts justice. The instrument panel has detail on both front and rear surfaces, the instrument dials themselves are recessed with slightly raised needles which with careful painting will look amazing, and on the rear surface have the back of the instruments with their associated wiring. The cockpit tub shows the same level of detail as the instrument panel and includes a realistic throttle and a mass of piping on the sides of the cockpit wall.

The detail set we received is designed to represent a late European aircraft with a Martin Baker seat, and this seat should be released on its own as I have rarely seen a resin casting of an ejector seat with such perfect detail on items like the seat straps and buckles.

As well as the super detailed cockpit you get a nice representation of the nose wheel bay and a long jet pipe that supplies more detail than the kit's example.

T-33/F-94 Wheel Bay Set

Product No: T.B.C

Designed for Hobbycraft/ Academy

Price: \$T.B.C

I must stress that this is a set still under development, but from what we can see in this sample this will be a must-have for anyone building the basic but nice Hobbycraft T-33. More news on this one once we have a production copy.



F-84F Detail Set European Version

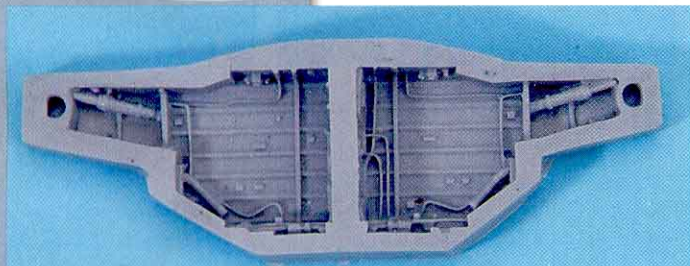
Conclusion

This is a very impressive start from AMS. The casting and attention to detail are among the best I have ever seen and the price is very reasonable for European modellers with the current exchange rate. The only criticism I have is the basic nature of the instructions, which do supply a written guide to the modifications required to the kit parts but leave you to do some research on painting the cockpit details. There is a nice link to www.seatejectcolor.com for detail photographs of the bang seat.

On their website AMS state they are looking at providing updates for all the Classic kits that have been ignored by other manufacturers, and I for one applaud this approach as I love the colourful aircraft of the 50s and 60s.

For details of their full range of F-84 sets, plus news of their future plans you can visit their website at <http://mysite.verizon.net/resqt29j>. Payment can be made via PayPal and if you have any queries you can contact AMS model by Email at hairold@verizon.net

SAMI



T-33/F-94 Wheel Bay Set

Coming to Greif

WHITE CAT MODELS

This manufacturer has a liking for Luft 46 subjects, and their latest is a proposed development of the Mistel family of composite aircraft. The Mistel mated a fighter on top of an unmanned bomber which was released as a primitive form of unguided missile. The only operational version of this aircraft used an Fw 190 and a Ju 88 but there were many other pairings considered and this conversion provides parts to produce the proposed long range Mistel combination of a He 177 and Fw 190.

The instructions in this set are bilingual German/English and include a number of black and white photographs to help the modeller complete the conversion.

He 177 Long Range Mistel

Product No: WCM 72004

Price: T.B.C

The resin parts supply the large warhead, four struts, and blanking plates for the 177 upper turret position. A small length of plastic rod is also supplied. The small decal sheet contains red 1 and 3 plus a small father and son badge carried by Mistels of 6./KG200.

The manufacturer recommends using the Revell Fw 190A-8 and of course the superb Revell He 177 for this conversion.

Conclusion

The resin will need some cleaning up before use, but is well cast, with no visible air bubbles or imperfections, and the decals are a nice touch. An



interesting choice of subject matter that would certainly draw some attention on the table at a model show or at a club night.

No UK stockist is known so if you are interested in purchasing this set contact Helmut Reichersdorfer of White Cat Models via Email at wcm@reichersdorfer.de.

SAMI

Coming Up Trumps

PAVLA

The latest releases from Pavla mainly concentrate on super detailing three aircraft types – Trumpeter's new Panther and Seahawk in 1/48 scale and the venerable 1/72 Viking by Hasegawa. All are produced in a cream resin and in most cases have a small black and white instruction sheet. One small problem is that many sets are supplied in a clear box with a card header and this is not the best way of protecting the thin resin parts as a number had broken in transit. In this eventuality you will simply need to be careful on opening the packages so that you retain any parts that have broken off, which can then be easily reattached with a small amount of super glue.

1/72

72043 Escapac 1E-1 £2.55

Ejection Seat for S-3A Viking. To fit Hasegawa/Revell kit.

72056 Grumman S2F-1 Tracker Cockpit + operator's station £15.50

To fit Hasegawa/Revell kit.

72057 Lockheed S-3A Viking Cockpit and operator's station set £15.50

To fit Hasegawa/Revell kit.

72058 Gannet T Mk 2: Cockpit detailing set £15.50

To fit Trumpeter kit.

72-78 F6F-5 Hellcat Engine set £4.85

To fit Hasegawa/Academy kit.

72-80 Lockheed S-3A Viking Bomb bay and wheel well £15.50

To fit Hasegawa/Revell kit.

72-79 Lockheed S-3A Viking Control surfaces £5.99

To fit Hasegawa/Revell kit.

1/48

C48012 Grumman F9F-2 Panther Cockpit £12.10

Includes complete tub, ejection seat, stick and rudder pedals as well as an instrument panel and coaming cast as one part. To fit Trumpeter kit.

48013 Sea Hawk FGA Mk 6 Cockpit set £12.10

For Trumpeter kit.

48-20 Upgrade set for Harrier & Sea Harrier

Includes starboard intake and exhaust nozzle. To fit Airfix kit.

48-21 Harrier & Sea Harrier Control surfaces

To fit Airfix kit.

U 48-22 F9F-2 Nose Gear Well and Fuselage Speed Brakes £12.10

Includes detailed wheel well, brake flaps and actuators. To fit Trumpeter kit.

U48-23 F9F-2 Rudder Elevators and Jetpipes £5.60

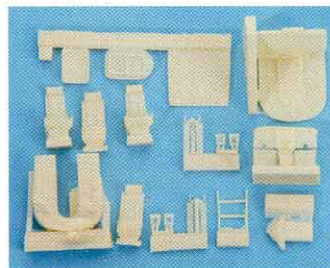
A simple 7-piece set that offers direct replacements for the kit parts. To fit Trumpeter kit.

U48-24 F9F-2 Fuselage Flaps £5.60

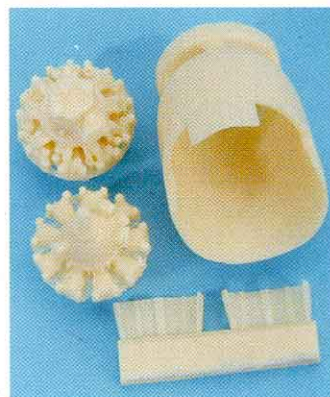
Let it all hang out with Pavla. Three nicely cast parts make up this effective set, again for the Trumpeter kit.

S48028 F9F Panther Ejection Seat £2.00

To fit Trumpeter kit.



72056 Grumman S2F-1 Tracker Cockpit + operator's station £15.50



72-78 F6F-5 Hellcat Engine set £4.85

48-25 Seahawk FGA Mk 6 Control surfaces £4.25

To fit Trumpeter kit.

48026 Stencil S-III-S (SJU-4) Ejector seat £2.00

For AV-8A Harrier, AV-8B Harrier, Alpha Jet.

48027 M.B Mk 10H Ejector seat £2.00

For Harrier FRS.I, F/A-2, Hawk T1a.

48028 M.B Mk 2D Ejector seat £2.00

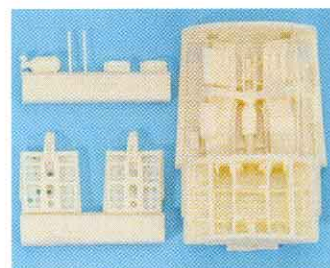
For Sea Hawk Mk. 2, 4, 5, 100, 101.

Conclusion

The quality of the resin is first rate and the sets for the Viking in particular will make a real show-stopping model from



48013 Sea Hawk FGA. Mk.6 Cockpit set £12.10



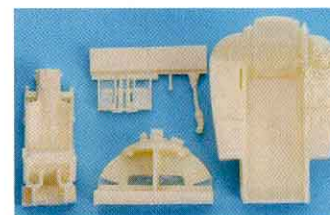
U 48-22 F9F-2 Nose Gear Well and Fuselage Speed Brakes £12.10



72058 Gannet T Mk 2: Cockpit detailing set £15.50



72-80 Lockheed S-3A Viking Bomb bay and wheel well £15.50

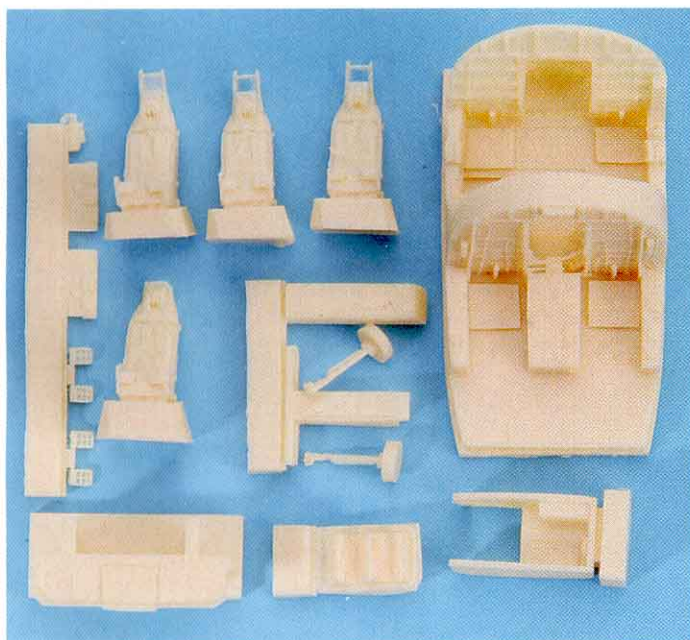


C48012 Grumman F9F-2 Panther Cockpit £12.10

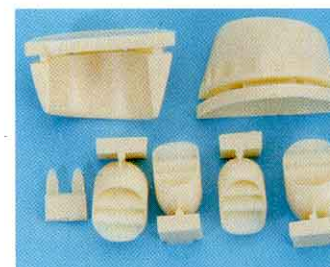
an older kit that is rarely seen on the competition table.

UK importer for Pavla's kits and accessories is Hannants.

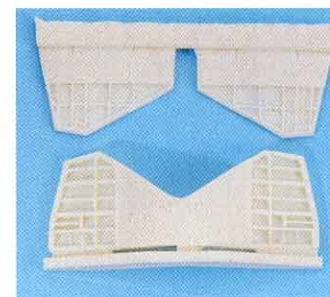
SAMI



72057 Lockheed S-3A Viking Cockpit and operators station set £15.50



48-20 Upgrade set for Harrier & Sea Harrier



U48-24 F9F-2 Fuselage Flaps £5.60

Big Figures

SMALL WORLD ACCS

This Italian company has just supplied us with three new additions to their range all with an Italian flavour. First of we have a very nice update for the recent 1/48 scale Italeri G.91R which of course will fit the earlier ESCI version. This late 70s kit is accurate but has virtually no detail in the cockpit and undercarriage bay, and this simple set supplies drop-in replacement resin parts for these areas, as well as separate flaps and weighted tyres.

One thing that had me scratching my head was the replacement nose cone. Apparently this is a different shape for the G.91 PAN version used by the Freccie Tricolori aerobatic team.



Italian Seated Pilots

A small instruction sheet clearly describes the construction process and also supplies a useful guide to the appropriate colours used inside the airframe.

In 1/72 scale we have three seated Italian second world war pilots, and we finish with a decal sheet for a selection of aircraft used by 53rd Stormo in 1/48 scale. This is a nice mini-theme sheet with individual markings for no fewer than 13 different aircraft and complete stencil data for one of each type. The instructions are in full colour and as well as the normal profiles there is a selection of close-up photographs to help you with the details

1/72

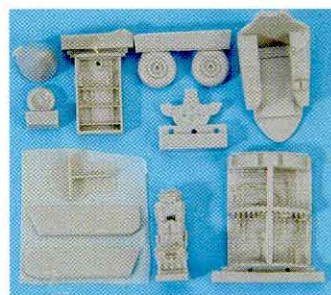
Italian Seated Pilots

Product No: SWA 72002
Designed for: N/A
Price: Euro 9.50

1/48

Fiat G.91R Detail Set

Product No: SWA 48001
Designed for: Esci/ Italeri N/A
Price: Euro 13.90



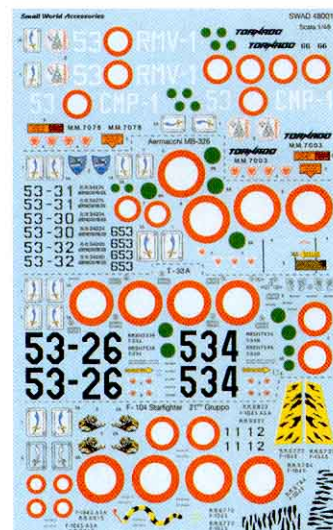
Fiat G.91R Detail Set

Italian Air Force 53 Stormo Decal Sheet

Product No: SWAD 48001
Price: Euro 12.50
1. T-33A, 53-26
2. T-33A, 534
3. MB-326E, 53-32
4. MB-326E, 53-30
5. MB-326E, 53-31
6. F-104S, 53-07, Tiger Meet 1980
7. F-104S 53-05
8. F-104S 53-20 Tiger Meet 1985
9. F-104S-ASA, 53-12
10 F-104S-ASA 53-11 Tiger Meet 1996
11. Tornado 53-CMP-1
12. Tornado 53-RMV-1

Conclusion

A very nice selection of subjects. I have already purchased an Italeri G.91 so I



Italian Air Force 53 Stormo Decal Sheet

can use the resin detail set, and regular readers know how I like mini themes so the decal sheet will also find a home in my pending pile.

The only downside is that there is no UK importer but thanks to the wonders of the Internet you can order this and many other Italian-orientated modelling products from www.Mondoridotto.it or by phone on 050 934257.

SAMI

Fair Exchange

CUTTING EDGE

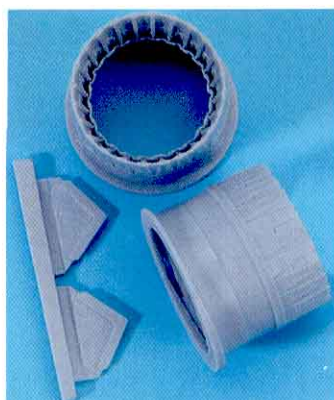
Three new sets from Cutting Edge this month. The first is a continuation of the 1/32 Phantom theme from last month's issue, and on the same subject is a new set to produce an early US Navy Phantom in 1/48 scale. Finally we have a new release in their seamless intake range.

1/48

F-4B/N Airframe conversion

Product No: CEC48508
Designed for Hasegawa
Price: \$48.99

Last month we covered the extensive 1/32 conversion sets Cutting Edge has produced to change a Tamiya C or J to



F-4 B/C/D/N J79 Nozzles

the B/N thin wing variant.

1/48 scale modellers have been well served as Hasegawa have, over the last 20 years, produced every variant of the Phantom, and their first kit was an F-4B Phantom so why do you need this set?

The first Hasegawa Phantom featured raised panel lines, which on later models have been replaced by some nice engraved detail. Even in newer reissues of the B/N variants you still have the raised detail, so if you want recessed panel lines on your early thin-wing Phantom you either have to do some major rescribing or purchase this conversion.

Start by cracking open a Hasegawa F-4J, to which you add the following new upper wing halves with the main wheel well bulges removed, new trailing edge flaps and ailerons, new speed brakes, narrow main wheels, and the fuselage extension for the early J79 engine nozzles. The instructions are very clear and concise and guide you through the conversion process using colour photographs of a model being assembled combined with informative text.

Although this is an extensive conversion it should be fairly straightforward as long as you follow the instructions explicitly, but if you buy it from Meteor and once it arrives decide it is beyond your capabilities, return it and they will refund you the cost of the set. You can't say fairer than that!



F-16 Small Mouth Intake

F-16 Small Mouth Intake

Product No: CEC48509
Designed for Hasegawa
Price: \$28.99

One of the big problems in producing a nice F-16 is getting an intake without a very visible seam line down the middle of the prominent intake. Cutting Edge has the answer with this seamless intake that provides a long inlet ending with a separate engine front. The intake features the top of the nosewheel bay and also has a flashed-over hole to take the thin blade area that goes through the centre intake. A really big plus is that this is moulded in white and just needs masking before spraying the external camouflage colour. For a really easy way to do this visit *Dave's Blog* on the Meteor web site.

1/32

F-4B/C/D/N J79 Nozzles

Product No: CEC32189
Designed for: Tamiya
Price: \$19.99

At first glance this is an addition to the B/N conversion set that we reviewed last

month, allowing those using the Tamiya J to produce the short nozzle used on the earlier models. But in fact this will also be very useful for anyone building a Tamiya C straight out of the box as the details on the resin parts are far superior to those provided in the kit.

As a bonus you also get a correction that relocates the vents above the exhaust pipes that are moulded in the wrong place on all of the Tamiya Phantoms.

Conclusion

The detail sets will be simple to use as minimal preparation is required and the appearance of your model will be enhanced considerably. The F-4 conversion would make a perfect introduction to more extensive conversions with the added benefit that there is a wide range of attractive aftermarket decal sheets produced for the early Phantom.

As I said last month the good news for European modellers is that the exchange rate is currently in our favour so visit Meteor Productions' website at www.meteorprod.com to see the full Cutting Edge range.

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MATADOR MODELS Airfield Accessories

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Refueller (early pattern)
conversion

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Modellers Datafile 11 Sea Harrier

written by Andy Evans

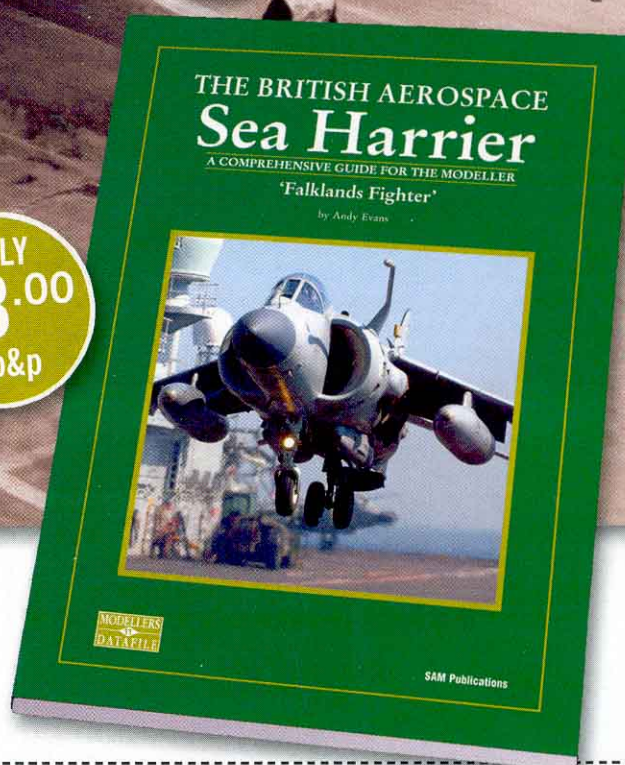
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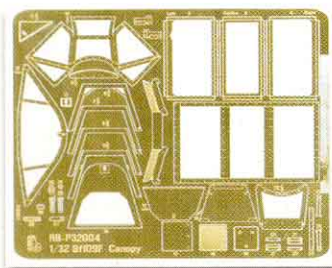
RB PRODUCTIONS

Radu Brinzan produces another amazing set for those who want to add the most amount of detail possible to a 109 in 1/32 scale. This set is suitable for the 21st Century Toys or the Hasegawa G if you backdate it to an F using the Aires conversion. This set would also be usable on an E4 or E7 as announced by Eduard for release later this year.

1/32

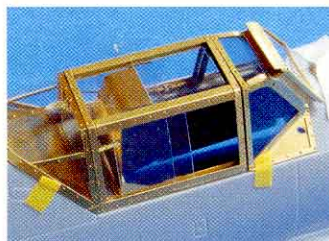
Bf 109F Canopy

Product No: RB-P 004
Designed for: 21 Century Toys
Price: 15 Euros



Conclusion

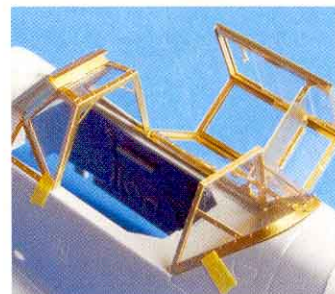
Once completed this set will supply a realistic canopy with functional sliding clear panels, but producing this level of detail from an etched brass set does require some practice. The quality of the instructions with their stage-by-stage photographic assembly makes this the



ideal starter for someone ready to move up to the next level.

Our thanks to Radu Brinzan for the review sample which can be purchased from RB Productions' ebay shop at <http://stores.ebay.ie/radub> or alternatively RB Productions range is also stocked by MDC models@ModelDesignConstruction.com in the UK and Eagle Editions eagle@eagle-editions.com in the United States.

SAMI



Uncle up the Ante

EDUARD

This month Uncle Eduard is treating us to brass for a diverse range of subjects, from Dragon's large scale P-51D Mustang to a 1/72 C-130. Only one mask set this month but it still got our Editor all excited as it was for the Special Hobby Skua which had only arrived the week before.

1/72

C-130H Hercules interior

Product No: SS 285 Zoom
Designed for: Italeri
Price: £3.70

EF 2000 Typhoon double seater

Product No: SS 289 Zoom
Designed for: Revell
Price: £3.70

B-26B/C Marauder interior

Product No: SS 292 Zoom
Designed for: Hasegawa
Price: £4.99

C-130H/J Hercules cargo floor

Product No: 72 470
Designed for: Italeri
Price: £15.50

C-130H Hercules exterior

Product No: 72 461
Designed for: Italeri
Price: £13.50

C-130H Hercules interior

Product No: 73 285
Designed for: Italeri
Price: £15.50

EF 2000 Typhoon double seater

Product No: 73 289
Designed for: Revell
Price: £10.50

B-26B/C Marauder interior

Product No: 73 292
Designed for: Hasegawa
Price: £13.50

C-130H Hercules seatbelts

Product No: 73 299
Designed for: Italeri
Price: T.B.C

1/48

Skua

Product No: EX 218 Mask
Designed for: Special Hobby
Price: £3.70

ACH-47A Chinook interior

Product No: FE 355 Zoom
Designed for: Italeri
Price: £4.99

Wellington Mk III interior

Product No: FE 379 Zoom
Designed for: Trumpeter
Price: £4.99



ACH-47A Chinook interior

ACH-47A Chinook interior

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Designed for: Italeri
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Wellington Mk III cockpit interior

Product No: 49 379
Designed for: Trumpeter
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Wellington Mk III mid-fuselage interior

Product No: 48 570
Designed for: Trumpeter
Price: £13.50

Wellington Mk III bomb bay

Product No: 48 571
Designed for: Trumpeter
Price: £31.99

1/32

P-51D Mustang exterior

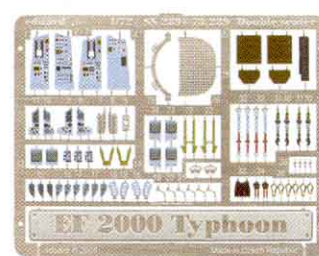
Product No: 32 168
Designed for: Dragon
Price: £T.B.C

P-51D Mustang interior

Product No: 32 569
Designed for: Dragon
Price: £15.50

P-51D Mustang wing armament

Product No: 32 597
Designed for: Dragon
Price: £11.75



EF 2000 Typhoon double seater

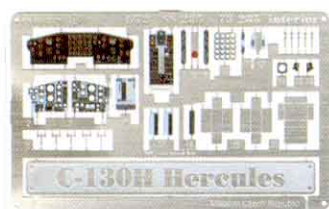
Conclusion

The Dragon P-51 is a contentious kit, and some may be reluctant to lavish further expense on it. The good news is that with a little work these sets will still fit the very old but accurate Hasegawa kit.

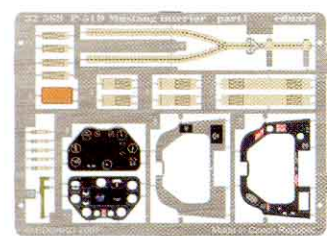
The Dragon P-51 might have a few problems, but the Italeri C-130 is one of those golden oldies kits that needs just a little help to make a real stunning model, and Eduard's latest will certainly provide lots of lovely detail.

Eduard's range is available in the UK from LSA Models and both branches of Hannants.

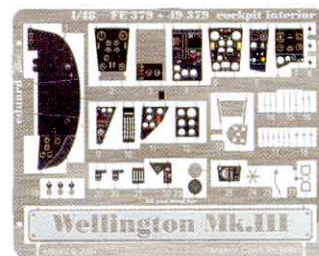
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C-130H Hercules interior



P-51D Mustang interior

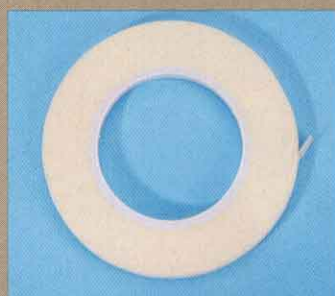


Wellington Mk III interior



B-26B/C Marauder interior

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5mm tape

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Have you ever tried to mask around a tight curve or on a concave or convex surface, or tried to reproduce a pin stripe on a civilian aircraft? Well here is the answer from Cammett - a range called 'Micro-tape,' which will solve all these problems. The product is available in a range of sizes: 0.5mm, 0.75mm, 1.0mm, 1.25mm, 1.50mm, 2.0mm, 3.0mm and 5.0mm. The smallest tape retails for £2.50 and the largest for £3.50 a roll.

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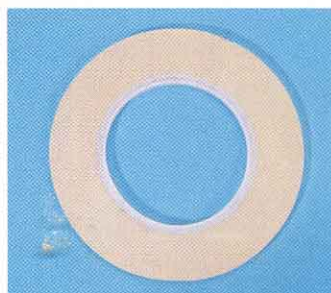
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these, and many other applications. The product is available from high street stores or on mail order direct from JML.

SAMI

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Alternative Alchemy

HAWKEYE

A few years ago one of my modelling nightmares was anything finished in natural metal. No matter what I tried, it never looked realistic. Then I discovered metal foils - a superb finish but a lot of

work, and I mucked up more foil than I successfully applied to a model. Then Alclad and SNJ were launched and suddenly realistic metal finishes became a possibility for any modeller in possession of an airbrush, and now even I could manage to produce a realistic finish.

But there was a disadvantage in that both of these products are based on cellulose and this meant that good ventilation was essential and the fumes could be quite overpowering.

Now from the producers of SNJ we have a new range of metallic finishes called Talon. These work in an identical way to the original SNJ but have an acrylic base. This means that there is no powerful smell. Application is by airbrush, and after about 10 minutes you can use SNJ polishing powders to buff the finish to a wide selection of shades. After an hour the finish will have fully cured and you can mask it

with a low tack masking tape.

The Talon range will consist of the same colours as the SNJ range but initially only Pewter, Steel and Aluminium are available.

Conclusion

I am still playing with the sample bottles but early results look very promising and with a threatened ban on cellulose based products it is nice to know that there is an alternative. Another benefit is that I will no longer get complaints about the strange smells omitting from my loft workshop after a prolonged airbrushing session.

I will report further, as I have a couple of NMF aircraft on the pending pile to feature in future issues of SAMI.

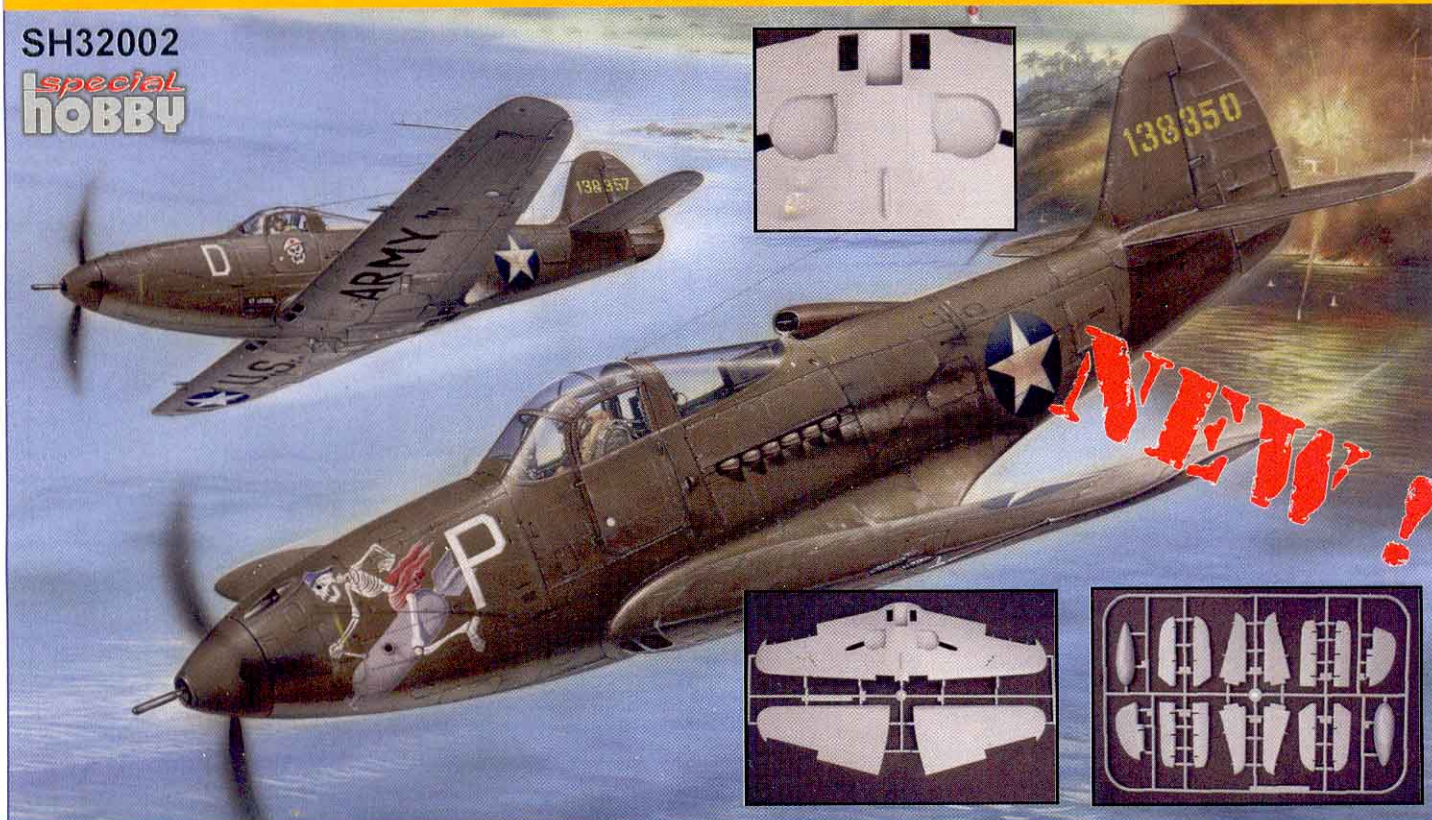
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Aftermarket decal sheets are another of those crucial aspects of the hobby that excite such attention. Reviews of decal sheets received not only provide information on available products, but can act as a useful reference point in its own right.

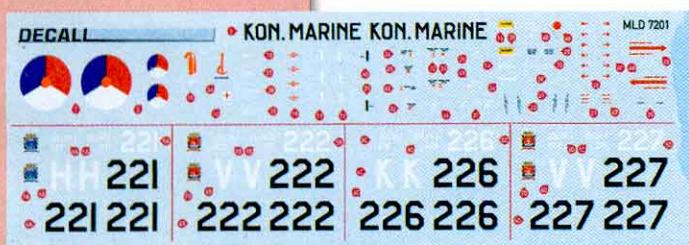
Scale Aviation Modeller international recognises the importance to the modeller of all aftermarket products, and our commitment to featuring new releases of decals, kits and accessories is absolute.

All items received for review will be treated with serious and professional consideration.

Kon Marine

DECAL

We have just received the latest release from this Dutch company and they have continued with their quest to supply markings for aircraft of the Dutch Kon. Marine. As on their first sheet the decals are printed using an ALPS printer so are more delicate than mainstream decals. They also have a one-piece carrier film - not a major problem, you just have to remember to cut close to the image before soaking in water. The instructions are in full colour and supply a clear placement guide for the decals. All of the options this month are finished in Extra Dark Sea Grey and Sky for which you are advised to use Humbrol 90/123 and Xtracolour X-607/ X-106.

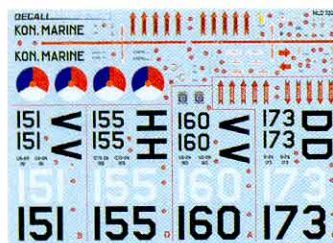


MLD 7201 Royal Netherlands Navy Agusta-Bell 204B

1/72

MLD 7201 Royal Netherlands Navy Agusta-Bell 204B

1. Agusta-Bell 204B (I)UH-1 221 VSQ 1. MVLK Hato.
2. Agusta-Bell 204B (I)UH-1 222 VSQ 7. MVLK Valkenburg.



MLD 7202 Royal Netherlands Navy Grumman S-2 Trackers

3. Agusta-Bell 204B (I)UH-1 226 MVLK De Kooy.
4. Agusta-Bell 204B (I)UH-1 227 VSQ 7. MVLK Valkenburg.

MLD 7202 Royal Netherlands Navy Grumman S-2 Trackers

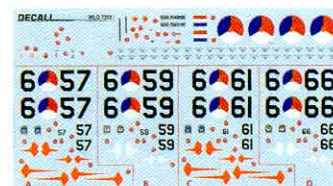
1. Grumman US-2N. 160 VSQ 320. MVLK Valkenburg.
2. Grumman US-2N. 151 VSQ 5. MVLK Valkenburg.
3. Grumman S-2A. 173 VSQ 4, on board aircraft carrier Hr. Ms. Karel Doorman.
4. Grumman ©S-2A. 155 VSQ 1. MVLK Hato.

MLD 7203 Royal Netherlands Navy Hawker Sea Hawk FGA.50 – pre-1959

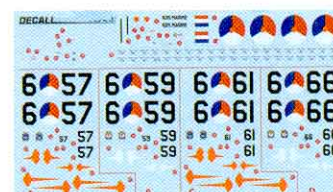
1. Sea Hawk FGA.50 6 + 57. VSQ 860, on-board aircraft carrier. Hr. Ms. Karel Doorman.
2. Sea Hawk FGA.50. 6 + 59 VSQ 3. MVLK Valkenburg.
3. Sea Hawk FGA.50. 6 + 61. VSQ 860, on-board aircraft carrier. Hr. Ms. Karel Doorman.
4. Sea Hawk FGA.50. 6 + 66. VSQ 3, MVLK Valkenburg.

MLD 7204 Royal Netherlands Navy Hawker Sea Hawk FGA.50 – post-1959

1. Sea Hawk FGA.50. 114. VSQ 860, on-board aircraft carrier. Hr. Ms. Karel Doorman.



MLD 7203 Royal Netherlands Navy Hawker Sea Hawk FGA.50 – pre-1959



MLD 4803 Royal Netherlands Navy Hawker Sea Hawk FGA.50 – pre-1959



MLD 7204 Royal Netherlands Navy Hawker Sea Hawk FGA.50 – post-1959



MLD 4804 Royal Netherlands Navy Hawker Sea Hawk FGA.50 – post-1959

2. Sea Hawk FGA.50. 117. VSQ 860, on-board aircraft carrier. Hr. Ms. Karel Doorman.
3. Sea Hawk FGA.50. 123. VSQ 3. MVLK Valkenburg.
4. Sea Hawk FGA.50. 131. VSQ 860. MVLK Valkenburg.

1/48

MLD 4803 Royal Netherlands Navy Hawker Sea Hawk FGA.50 – pre-1959

As listed above for MLD 7203 in 1/72 scale.

MLD 4804 Royal Netherlands Navy Hawker Sea Hawk FGA.50 – post-1959

As listed above for MLD 7204 in 1/72 scale.

Conclusion

If you are looking for something different or have a liking for Dutch Naval subjects this is the manufacturer for you. Future projects include markings for Dutch BO-105 in all three major scales as well as the previously announced sheets for the F-86Fs and F-100Cs of the Dutch based 32 TFS of the USAF.

These sheets can be ordered direct from Decall, J.C. Trelouw, Morgan 16, 3763 WH Soest, Netherlands, priced 16 Euros for 1/72 scale and 20 Euros for 1/48, including international postage. Enquires can be made via email to decall@hetnet.nl

SAMI

Sea Harrier End of an Era

SAM PUBLICATIONS

It has been 25 years since the combat debut of the Sea Harrier over the Falklands and in the intervening years it has seen service in the Balkans and over Iraq before finally retiring last year from the Royal Navy. Now the publishers of your favourite modelling magazine have produced a fitting tribute in this decal sheet.

Printed by Fantasy Print shop, this is a large sheet, almost A4, and packed with perfectly printed markings in both 1/72

and 1/48 scale as you can see in the accompanying illustration.

The decal sheet supplies full stencil data for one option but only one style of national insignia - the faded blue and red type used during the Falklands war - not a major problem as the other styles are included on most kits' decal sheets. There is large number jungle, as well as some made-up serial numbers, so with a little cutting you can produce a model of any Sea Harrier in service with the Royal Navy. Also supplied are a large number of crew names and the various squadron insignia.

Conclusion

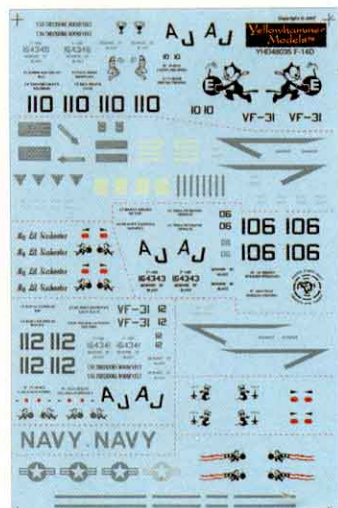
This is a must-have sheet for anyone interested in the Sea Harrier, but the bad news is that you cannot purchase it at your local model shop. The good news is that it is included free with the new Sea Harrier Datafile, which should be available by the time you read this. To order your copy see the advert in this issue or visit the SAM Publications web site at www.sampublications.com

SAMI

Remember Poindexter

YELLOWHAMMER

One of the oldest squadron markings in the world, the Felix the Cat emblem has been on many classic fighters since the 1930s and most of them have come from the Grumman Ironworks. The final product from Grumman was the F-14 Tomcat and over the last few years there have been some colourful retirement schemes as this type has left service to make way for the Super Hornet.



YHD48035 Tomcat Sunset Part 1

Yellowhammer continues to cover the final schemes that VF-31 applied to their Tomcats and of course Felix is normally present in one form or another on all of the airframes.

1/48

YHD48035 Tomcat Sunset Part 1

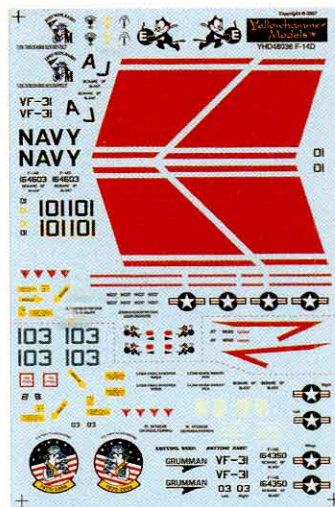
1. F-14D, BuNo 164346, VF-31, AJ/110, USS Theodore Roosevelt, 28 Jul 2006
2. F-14D, BuNo 164343, VF-31, AJ/106, USS Theodore Roosevelt, 28 Jul 2006
3. F-14D, BuNo 164341, VF-31, AJ/112, USS Theodore Roosevelt, 28 Jul 2006

YHD48036 Tomcat Sunset Part 2

1. F-14D Tomcat. VF-31 Tomcatters, Cdr Jim 'Puck' Howe. September 8, 2006. NAS Oceana, VA.
2. F-14D Tomcat. VF-31 Tomcatters. CDR Great Knepper. September 20, 2006. NAS, Oceana, VA.

YHD48037 Tomcat Sunset Part 3

1. F-14D Tomcat. VF-31 Tomcatters. CDR Curt 'Opie' Seth. September 20, 2006. NAS Oceana, VA.
2. F-14D Tomcat. VF-31 Tomcatters.

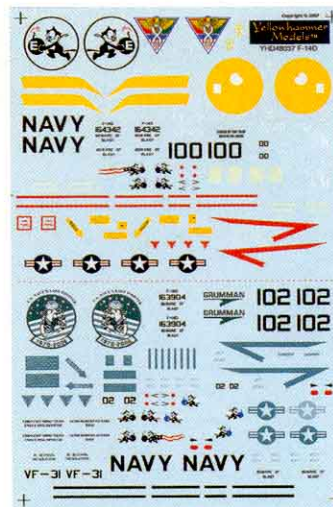


YHD48036 Tomcat Sunset Part 2

Capt. Dan 'Dix' Dixon. September 8, 2006. NAS Oceana, VA.

Conclusion

I like my themes, and I can think of at least two using this sheet. The most obvious is a whole table featuring the many colourful Tomcat Retirement schemes, but you could also do one on Felix the Cat of VF-31, which would make a real eye-catching display from biplanes to the Tomcat. The decals are well printed and include all the major stencilling for both options on each



YHD48037 Tomcat Sunset Part 3

sheet, while the instructions feature colour profiles, which clearly show where the decals are to be placed.

If you like the Tomcat you must have one of these sheets, and I am hoping that Yellowhammer might consider further sheets featuring the other aircraft that have carried the Felix emblem through the ages.

Yellowhammer decals can be purchased online from Meteor productions at www.meteorprod.com

SAMI

Going for the One

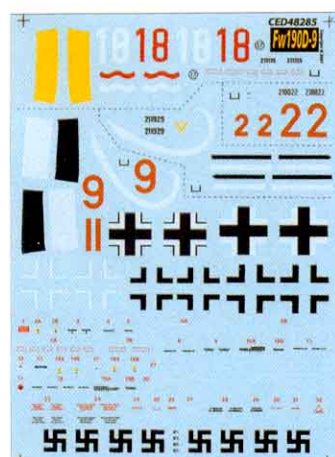
CUTTING EDGE

The latest sheets from Cutting Edge return to the ever popular aircraft of the Luftwaffe in 1/48 and 1/32 scale. All three sheets come with full colour instructions that clearly guide you to the correct positioning of the decals. Each sheet provides basic stencil detail and national insignia as well as the individual aircraft markings and badges.

1/48

CED48285 Fw 190D-9 Dazzling Doras

The Dora can hardly be described as the



CED48285 Fw190D-9 Dazzling Doras

most colourful of Luftwaffe types but Cutting Edge have found some interesting schemes for this sheet including an aircraft with very attractive natural metal lower surfaces.

1. Fw 190D-9, WNr 211115, Red 18, I/EKG(J), Pilsen, 1945
2. Fw 190D-9, WNr 211929, Stab/JG 6, Reichenberg, 1945
3. Fw 190D-9, WNr 210xxx, Brown 9, 7/JG 26, Nordhorn-Klausheide, 1945
4. Fw 190D-9, WNr 210222, Brown 2, 7/JG 26, Schesig, 1945

1/32

CED32082 Ju 87D/G Stuka Attack Pt1

This set provides markings for three Stukas, two in the standard splinter camouflage and the other in an overall whitewash finish that has started to wear away to reveal the splinter camouflage. This set also includes a small BlackMagic mask to reproduce the yellow V that is carried under the wings of the second option. A small correction sheet supplies alternative squadron badges and squadron codes as on the original sheet this may have been printed incorrectly. This error was caused by the use of a period colour photograph as a reference which subsequently proved to have been touched up, potentially altering the colours.



CED32082 JU 87D/G Stuka Attack Pt1

1. Ju 87D-1, J9+CK, III./St.G 1, Winter 1942, Russia
2. Ju 87D-8, WNr 148896, H5+F, Poss. NSGr.10, Surrendered at Furth, Germany, 1945
3. Ju 87G, WNr 494230, S7+BU, 10./SG.3, 1944, Riga, Latvia

CED32083 Ju 87D Stuka Attack Pt2

Two more Stukas, one in splinter camouflage, the second in a heavily weathered winter white wash.

1. Ju 87D-1, T6+AN, II./St.G 2, Spring 1942, Russia, Knight on Horse badge on nose.
2. Ju 87D-1, T6+DK, II./St.G 2, March 1942, Russia, Scotty dog badge under canopy



CED32083 JU 87D Stuka Attack Pt2

Conclusion

These are typical Cutting Edge decals providing some interesting alternatives to the kit's markings, but there is a small bonus if you have access to the Internet. If you visit Dave's Blog on the Meteor Productions' web site you will find a very detailed description of the Ju 87 colours supported by photographic evidence.

Cutting Edge products are available in the UK from Hannants or online direct from Meteor Productions.

SAMI

Light My Fire

BRIGHT SPARK

Bright Spark decals have produced two of those ever useful sheets that supply national markings for countries that are normally overlooked by larger companies.

Both sheets come with full instructions which give information on the aircraft that they can be applied to, and there is a broad spectrum of types covered, from interwar biplanes to supersonic jets fighters. Each sheet supplies two decals, one contains the national insignia and the other contains a number jungle for the serial numbers.

1/72

BS4 Alpine Air Arms Switzerland

This sheet supplies a large number of

Swiss white crosses, most with a red background but some without. A second sheet supplies codes for numerous serial numbers.

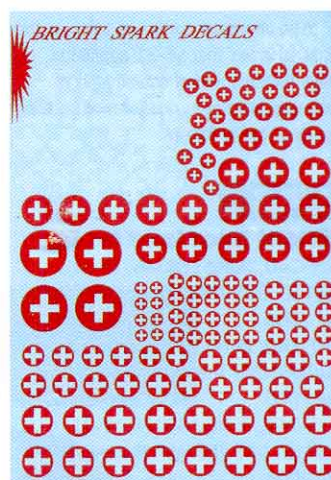
BS5 Alpine Air Arms Austria

This sheet supplies 157 examples of the distinctive Austrian national markings plus six national flags. The second sheet supplies the numbers, and Austrian Air Force titles as seen on the C-130 Hercules.

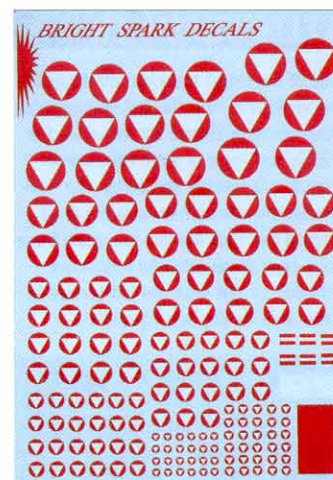
Conclusion

These decals are available in very limited quantities so if the subject matter appeals to you send a cheque for £9.99 per sheet made payable to M A Burton at 2 Gloucester Road, Waterlooville, PO7 7BJ.

SAMI



BS4 Alpine Air Arms Switzerland



BS5 Alpine Air Arms Austria

What's New Pussycat?

OWL DECALS

Czech manufacturer Owl have been steadily increasing their catalogue in recent months, and a selection of unique and very welcome accessories are opening up new possibilities for modellers of Luftwaffe subjects almost as fast as kit producers are bringing out new releases upon which to lavish them. Owl's speciality is night fighters, and their decals and conversion parts are currently one of the fastest selling aftermarket ranges.

Owl's decals currently span four scales. A number of the sheets are identical in both 1/72 and 1/48, and the one-shot 1/32 releases are a particularly good idea for those modellers with neither the time nor space to build too many models in this scale. Instruction sheets are in full colour and also refer the modeller to those items in the Owl range necessary to convert a model to the subject matter in hand.



72011 Ju 88 Nachtjäger £5.85

1/72

72011 Ju 88 Nachtjäger £5.85

1. Ju 88C-6, R4+HH, 1./NJG 2, Catania 1942. RLM 22 all-over with yellow undersides to the cowlings
2. Ju 88G-1 D5+PH, 1./NJG 7, Kastrup, Denmark, 1945. RLM 76 all-over with RLM 75 mottle on upper surfaces
3. Ju 88G-6, B4+FA, NJ-Staffel Norwegen, Mannheim Sandhofen. RLM 76 all-over with RLM 75 mottle on upper surfaces, with RLM 22 underside to starboard wing

1/48

48011 Ju 88 Nachtjäger £9.99

Options on this sheet are identical to those on 72011.

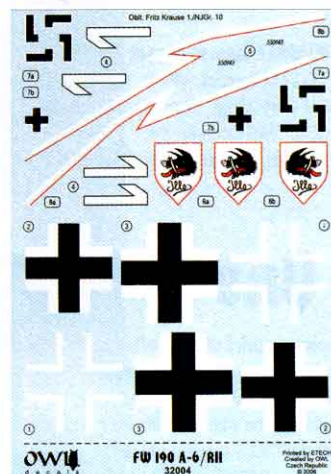
- R48001 Fw 190A-8/R11 £3.35
This neat little sheet features markings for the aircraft of Ofw. Gunther Migge of 1./NJGr. 10 Werneuchen 1944. This machine is equipped with FuG 217-J-2 Neptune radar. RLM 76 with RLM 74/75 uppers and a mottle of RLM 75 on the vertical tail surfaces.
- R48002 Fw 190A-6/R11 £3.35
This neat little sheet features markings for the aircraft of Oblt. Fritz Krause of 1./NJGr. 10 Werneuchen 1944. This machine is equipped with FuG 217-J-1 Neptune radar. RLM 76 with RLM 74/75 uppersurfaces.

1/32

32003 Wilde Sau Part 2 £6.30

Two interesting specimens from the nightlife of late 1943war Germany.

1. Fw 190A-5, 10./NJG 3, D5+XV, Aalborg West, early 1944. RLM 76



32004 Fw 190A-6/R11 £4.60

undersurfaces with RLM 74 uppers and a small mottle of 74/75 on the vertical tail surfaces.

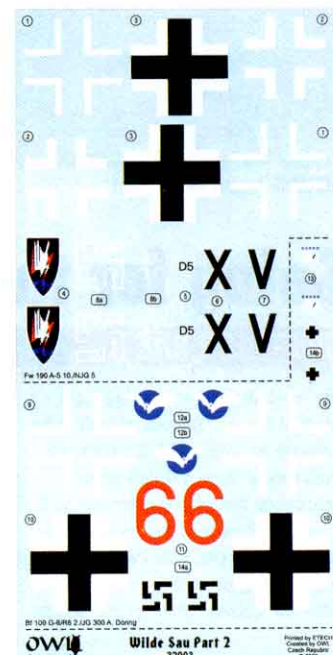
2. Me Bf 109G-6/R6, 2./JG 300, Ofw. Arnold Doring, Bonn Hangar October 1943. This machine has RLM 22 undersurfaces, and a scribe of the same obscuring most of the upper, although glimpses of standard 74/75/76 camo can be seen in places. National insignia and codes are provided, but these will need to be partially obscured with the top coat of RLM 22.

32004 Fw 190A-6/R11 £4.60

This is a scaled-up edition of Fritz Krause's aircraft as depicted in #R48002.

Owl are to be commended not only for the interesting choices of subject matter, but also for their attention to the two opposite ends of the scale – 1/144 and 1/32, both of which are deserving of more attention from the marketplace.

Hannants are currently importing the



32003 Wilde Sau Part 2 £6.30

range to the UK, and Squadron in the US carry the decals. Meanwhile a glance at www.owl.cz will allow the reader to enjoy the full range of useful items currently on offer, and for further inspiration Willi Reschke's evocative memoirs of his service with JG 301/302, 'Wilde Sau' can be highly recommended.

SAMI

SHAR Wars

MODEL ALLIANCE

BAe Sea Harrier Part 1

1:72 scale NA 72153 £9.00

1:48 scale MA 48153 £13.00

This latest Model Alliance decal sheet covers the early markings applied to the Sea Harrier FRS.1, and the Indian Navy's FRS.51, T.60 and T.4(I). The decals are crisply printed and the colours hold up well to the original markings, and there is a choice of aircraft from each of the early SHAR units in their pre-Falklands Extra Dark Sea Grey and White including the trials unit, and the Indian Navy in their EDSG and Dark Sea Grey/Medium Sea Grey finishes. The booklet that accompanies the decal sheet is comprehensive with plenty of information available on placement,

colour scheme and anomalies for the individual aircraft.

The Canberra has finally left service with the RAF after a long and distinguished career and the final PR.9 was painted in an attractive retirement scheme on top of the aircraft's normal beige and grey. The decals supply all the special markings with the small squadron badges printed in two parts to insure perfect registration. Also supplied are the national insignia and aircraft codes for the considerable stencil detail carried on a PR.9, together with the wing walkways.

At the time of writing no injected moulded kit of this version is available though both Airfix and Classic Airframes have announced a PR.9 in 1/48 and a smaller scale version will be coming from the Czech Republic in due course.

1/72

MAS-729032 Canberra PR.9 End of an Era 1916-2006

1. BAE Canberra PR.9, XH134, 39 PRU, RAF Marham, 2006.

1/48

MAS-489032 Canberra PR.9 End of an Era 1916-2006

Listed exactly as above for 1/72 scale MAS-729032.

Conclusion

This sheet was produced with the help of the designer of the full sized scheme so its accuracy is guaranteed, and as the PR.9 is my favourite Canberra variant I think I will be investing in a Classic Airframes example so I can use this



sheet as soon as it is released.

These sheets and others from the range are available direct from the manufacturer at www.theaviationworkshop.co.uk.

SAMI

Home from the Hill

RAFDEC

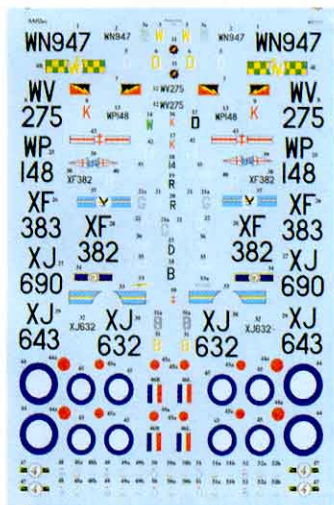
The latest decal sheet from RAFDEC is a real masterwork on the Hawker Hunter, and although all options in standard RAF colours there is enough squadron heraldry on display to make for an interesting line-up. The instructions are extensive, covering 9 pages of A5 with full colour profiles, and the decals themselves are printed to a very high standard.

A small bonus section supplies squadron badges for six units, and these are made up from three pieces each, to insure perfect registration on such small images.

1/72

RF7213 Hawker Hunters

1. Hawker Hunter FGA.9, XJ643, 'D' of 28 Squadron, RAF based at Kai Tak, Hong Kong, Jan 1967.
2. Hawker Hunter FGA.9, XJ 632, 'B'



RF7213 Hawker Hunters

of 208 Squadron, RAF based at Muharraq, Bahrain, 1967.

3. Hawker Hunter FGA.9, XJ690, 'G' of 20 Squadron, RAF based at Tengah, Singapore, 1966.
4. Hawker Hunter F.6, XF383, '14' of 145 Squadron, 229 OCU, RAF based at RAF Chivenor, 1961.
5. Hawker Hunter F.6, XF382, 'R' of Fighter Combat School, Central Flying Establishment based at RAF West Raynham, 1961.
6. Hawker Hunter F.2, WN947, 'N' of 257 Squadron, RAF based at RAF Wattisham, 1956.
7. Hawker Hunter F.5, WP148, 'K' of 41 Squadron, RAF based at RAF Biggin Hill, 1956.
8. Hawker Hunter F.4, WV275, 'D' of 4 Squadron, RAF based at RAFG Jever, 1955-57.

Bonus Section

Squadron and Unit Crests

1. Avro Lancaster PR.I of 683 Squadron.
2. Bristol Belvedere of 26 Squadron.
3. Gloster Meteor of RAF Syt.
4. Piston Provost of 6 Flying Training School.
5. Piston Provost of 22 Flying Training School.
6. Piston Provost of 2 Flying Training School.

Conclusion

The Revell Hunter is a very nice kit and these decals will make it very hard to build just one as there are so many interesting options. I personally like the fighter combat school Hunter with its large areas of day-glo across the spine, tail and wing.

This superb sheet is available from Hannants for a very reasonable £9.00.

SAMI

New Tricks

FREIGHTDOG

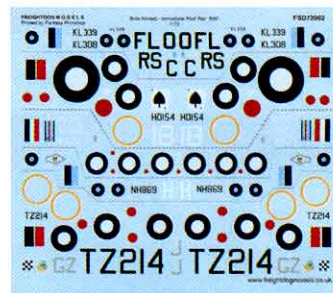
1/72

1/48

FSD72002/FSD48001 Brits Abroad Immediate Post-War RAF

Another imaginative release from Freightdog tackling lesser-known and unfrequented areas of classic British aviation. 1/72 sheet is priced at £5.50, 1/48 at £9.00. printed by Fantasy Printshop to a high standard, this sheet comes with full colour instructions.

1. Republic Thunderbolt Mk. II KL308, 30 Sqn, Baigatchi, India 1945
2. Republic Thunderbolt Mk. I HD154, 73 OTU, Fayid, Egypt 1946



FSD72002/FSD48001 Brits Abroad Immediate Post-War RAF

3. Supermarine Spitfire FR. Mk.XIVe, NH869, 28 Sqn, Kuala Lumpur 1946
4. Supermarine Spitfire F.Mk. XIVc, RN135, 17 Sqn, Miho, Japan, 1946
5. Supermarine Spitfire FR. Mk.XIVc, RN185, 17 Sqn, Miho, Japan, 1946

SAMI

Space Pucara

OTTER

1/72

72001 IA-58A Pucara Argentina

72002 IA-58A Pucara International

These sheets have been passed on to us through the auspices of Group Editor Neil Robinson, and originate from Argentinian artist Diego Rohas, whose authoritative artwork on the subject featured in Model Aircraft Monthly's recent Falkland's special. Marketed under the name 'Otter Decals,' the sheets cover the Pucara in

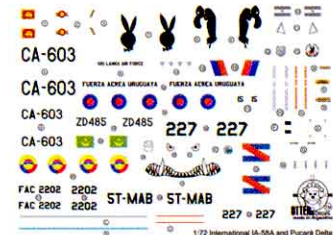
both Argentinian and international service – including British – and will be a useful addition to the currently small catalogue of aftermarket products associated with this type.

Readers can be assured the artist knows his stuff, and the quality of the sheet is impeccable. Little in the way of details has so far been provided, but interested parties should contact diegor1967@hotmail.com The price is around US\$10.

SAMI



72001 IA-58A Pucara Argentina



72002 IA-58A Pucara International

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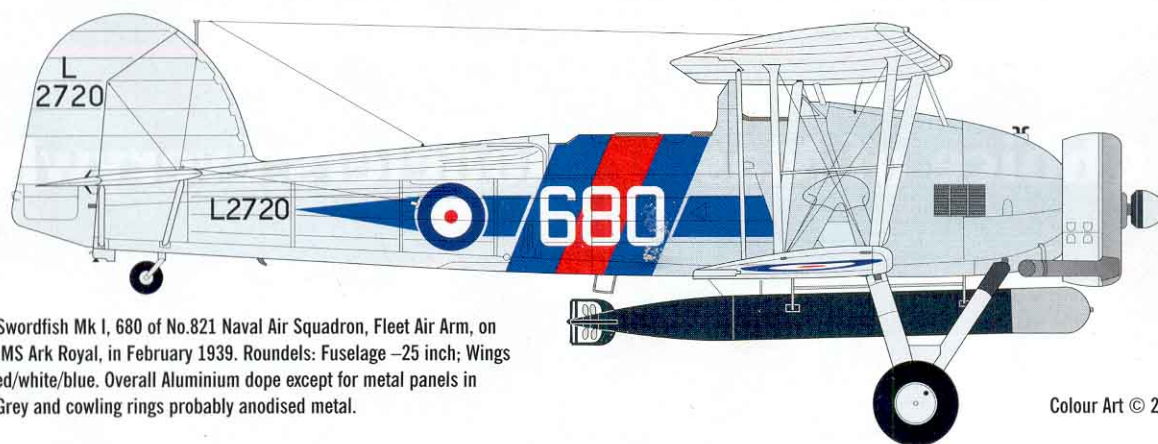
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L2720, Swordfish Mk I, 680 of No.821 Naval Air Squadron, Fleet Air Arm, on board HMS Ark Royal, in February 1939. Roundels: Fuselage – 25 inch; Wings – 52" red/white/blue. Overall Aluminium dope except for metal panels in Cerrux Grey and cowl rings probably anodised metal.

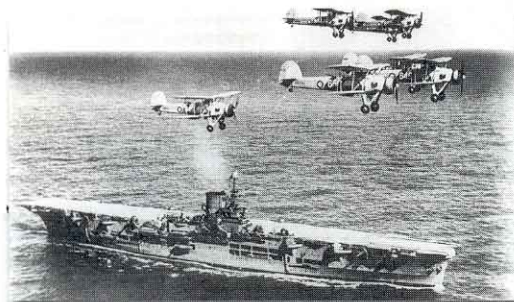
Colour Art © 2007 David Howley

The Other TSR.2

Fairey's Big Fish – the multi-faceted Swordfish

Fairey initially conceived the Swordfish as a private venture in response to Air Ministry requirements for a Spotter-Reconnaissance plane, a specification that subsequently called for an additional torpedo bomber role. Thus the prototype TSR II ('Torpedo-Spotter-Reconnaissance' - TSR I was the private venture prototype), first flew on April 17, 1934. It was a large biplane with a metal fabric-covered frame, and incorporated manually folding wings for carrier use. An order was placed in 1935 and the aircraft entered service in 1936, replacing the Fairey Seal in the torpedo bomber role.

By 1939 the Royal Navy had thirteen squadrons equipped with the Swordfish Mark I. There were also three flights of Swordfish with floats, for use off catapult-equipped warships. One such, from HMS Warspite spotted fall of shot



A flight of Swordfish seen airborne over HMS Ark Royal, circa 1939



Unidentified torpedo-armed Swordfish in pre-war markings

during the Second Battle of Narvik and subsequently sank the U-boat U-64.

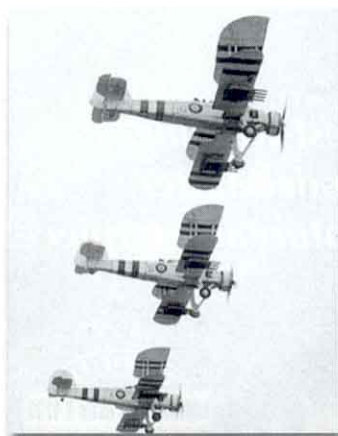
Swordfish subsequently flew combat operations from MAC ships - merchant aircraft carriers. Twenty of these civilian cargo ships were modified to carry three or four aircraft each on anti-submarine duties with convoys. Three of these were Dutch manned, flying Swordfish from 860 (Dutch) Squadron.

The Swordfish's crew varied according to mission. It was equipped to carry three, but on long range missions a fuel tank would be installed, occupying most of the rear compartment, to the discomfiture of the remaining crewman. The Swordfish's primary weapon was the torpedo, but the aircraft's low speed, and the need for a long straight approach, made it difficult to deliver against well defended targets. However, Swordfish flying from HMS Illustrious carried out the aircraft's most famous operation on November 11 1940 at Taranto, sinking or disabling three Italian battleships and a cruiser, and effectively putting an end to the Italian Navy as a force to be reckoned with in the Mediterranean theatre. Swordfish worked alongside Albacores flying anti-shipping sorties from Malta, and this interdiction played a major part in depriving the

hard-pressed Afrika Korps of vital fuel and munitions, significantly affecting the campaigns in North Africa.

In May 1941 a Swordfish strike from HMS Ark Royal was instrumental in

damaging the German battleship Bismarck, preventing it from escaping back to France. The low speed of the attacking aircraft may have acted in their favour, as they were too slow for the fire-



Three rocket projectile Fairey Swordfish during a training flight from St Merryn Royal Naval Air Station. Note the invasion stripes carried for the Normandy landings on the wings and fuselage of the aircraft





L2720, Swordfish Mk I, 680 of No.821 Naval Air Squadron, Fleet Air Arm, on board HMS Ark Royal, in February 1939. Roundels: Fuselage – 25 inch; Wings – 52" red/white/blue. Overall Aluminium dope except for metal panels in Cerrux Grey and cowling rings probably anodised metal.

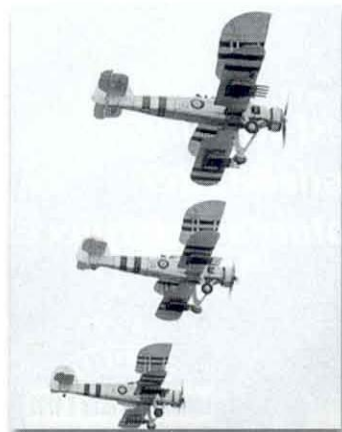
Colour Art © 2007 David Howley

The Other TSR.2

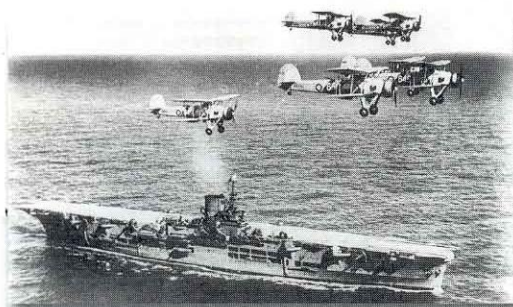
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Tamiya 1/48th Swordfish Mk 2

The kit is well presented in Tamiya's standard box. The package is quite full of light grey plastic parts with one clear sprue, another bag with two soft plastic rings, a small decal sheet and a 1/48th scale set of camouflage drawings.

I was really hoping that this release would be the Mk 3 with the large radome but it isn't, so what changes have Tamiya made? The lower wings now represent the metal skinned ones and there are the strut mounted radar antenna (although the interior doesn't have the 'gubbins' to operate the radar). The extra fuel tank, large oil cooler and the lower fuselage flare tubes complete the changes. So, radome next time Tamiya?

Construction

With a Tamiya kit if it doesn't fit you must be doing something wrong, but I tried and tried and still got a slight gap when closing the fuselage aft of the gunner. I didn't on a previous build so I don't know what went wrong, - it's as if the cockpit module is just too wide. Other than that there are no real problems to report. All joints fall on panel lines and considering the complex shape of the real item the parts all fit well. There is some great detailing in the cockpit, although I did add an Eduard Sutton harness for the pilot and seat straps for the observer.

The new parts fit as well as the originals do and despite the gap it was a pleasure to build.

I completed the fuselage then deviated from the instructions and completed the outer bi-plane



assemblies, gluing them to the central upper wing section then installed this quite large assembly to the fuselage. This allows you to lie the wings flat and glue them without undue strain on the mating faces. Once the wings had set it was upside down and on with the undercarriage, again no problems encountered. One small tip - you can fit the entire engine assembly after spraying... it's a nightmare masking all those pipes!

On my previous floatplane build (I folded the wings and hid the cockpit hence this build has spread wings) I used stretched sprue for the rigging but I thought I would give the Tamiya rigging sheet a go. This brings me to another issue that is not a fault of the kit, but has been mentioned before. The etched rigging didn't turn out, as I expected, being a bit slack and not straight, but suddenly, bingo-bongo, a more-or less complete stringbag appeared.

I did not fit the machine guns, as it was apparently not unusual for escort

missions to be flown by just the pilot, extending flight endurance by using the extra fuel tank and weight saving on the guns and two less crew.

Colour Options

The decal sheet covers aircraft from three RN squadrons, one operating from a MAC Escort Carrier branded 'Merchant Navy.' General stencils for the walkways etc. are included but you have to paint the two wing-tread areas.

I didn't use any of the schemes provided as I fancied the D Day machines of 816 Squadron shown on page 49 of Squadron Publication's Swordfish in Action. The D Day stripes give the old girl a certain character. I used the kit decals to represent one of

these aircraft and they went on well without silvering.

Conclusion

Another high quality kit. Price will always be subjective, and I know I added to it by spending £7.99p on the etched sheet, but building work involved is minimal, the filler used was only a smear, and I spent more time painting than making it, so it gets my vote as a 'wet weekend' kit. Another big well done to Tamiya!

Gordon Scott



Tamiya 1/48th Swordfish Mk 2

The kit is well presented in Tamiya's standard box. The package is quite full of light grey plastic parts with one clear sprue, another bag with two soft plastic rings, a small decal sheet and a 1/48th scale set of camouflage drawings.

I was really hoping that this release would be the Mk 3 with the large radome but it isn't, so what changes have Tamiya made? The lower wings now represent the metal skinned ones and there are the strut mounted radar antenna (although the interior doesn't have the 'gubbins' to operate the radar). The extra fuel tank, large oil cooler and the lower fuselage flare tubes complete the changes. So, radome next time Tamiya?

Construction

With a Tamiya kit if it doesn't fit you must be doing something wrong, but I tried and tried and still got a slight gap when closing the fuselage aft of the gunner. I didn't on a previous build so I don't know what went wrong, - it's as if the cockpit module is just too wide. Other than that there are no real problems to report. All joins fall on panel lines and considering the complex shape of the real item the parts all fit well. There is some great detailing in the cockpit, although I did add an Eduard Sutton harness for the pilot and seat straps for the observer.

The new parts fit as well as the originals do and despite the gap it was a pleasure to build.

I completed the fuselage then deviated from the instructions and completed the outer bi-plane



missions to be flown by just the pilot, extending flight endurance by using the extra fuel tank and weight saving on the guns and two less crew.

Colour Options

The decal sheet covers aircraft from three RN squadrons, one operating from a MAC Escort Carrier branded 'Merchant Navy.' General stencils for the walkways etc. are included but you have to paint the two wing-tread areas.

I didn't use any of the schemes provided as I fancied the D Day machines of 816 Squadron shown on page 49 of Squadron Publication's Swordfish in Action. The D Day stripes give the old girl a certain character. I used the kit decals to represent one of

these aircraft and they went on well without silvering.

Conclusion

Another high quality kit. Price will always be subjective, and I know I added to it by spending £7.99p on the etched sheet, but building work involved is minimal, the filler used was only a smear, and I spent more time painting than making it, so it gets my vote as a 'wet weekend' kit. Another big well done to Tamiya!

Gordon Scott

SAMI

assemblies, gluing them to the central upper wing section then installed this quite large assembly to the fuselage. This allows you to lie the wings flat and glue them without undue strain on the mating faces. Once the wings had set it was upside down and on with the undercarriage, again no problems encountered. One small tip - you can fit the entire engine assembly after spraying... it's a nightmare masking all those pipes!

On my previous floatplane build (I folded the wings and hid the cockpit hence this build has spread wings) I used stretched sprue for the rigging but I thought I would give the Tamiya rigging sheet a go. This brings me to another issue that is not a fault of the kit, but has been mentioned before. The etched rigging didn't turn out, as I expected, being a bit slack and not straight, but suddenly, bingo-bongo, a more-or less complete stringbag appeared.

I did not fit the machine guns, as it was apparently not unusual for escort



General characteristics

Crew: Three (pilot, observer, and radioman/rear gunner)
 Length: 35 ft 8 in (10.87 m)
 Wingspan: 45 ft 6 in (13.87 m)
 Empty weight: 4,195 lb (1,900 kg)
 Loaded weight: 7,720 lb (3,500 kg)
 Maximum speed: 138 mph at 5,000 ft (222 km/h at 1,500 m)
 Range: 546 miles combat;
 1,025 miles ferry (879 km/1,650 km)
 Service ceiling: 19,250 ft (5,870 m)

Armament

1x 0.303 in (7.7 mm) Vickers machine gun in engine cowl
 1x 0.303 in (7.7 mm) Lewis or Vickers machine gun in rear cockpit
 1x 1,670 lb (760 kg) torpedo or 1,500 lb (700 kg) mine
 8x 60 lb (27 kg) RP-3 rocket projectiles (Mk.II and later)

control predictors of the German gunners, whose shells exploded far in front of them. The Swordfish also flew so low that most of the Bismarck's flak was unable to hit them. The aircraft scored two hits, one that did little damage but another that succeeded in disabling the Bismarck's rudder, preventing it from manoeuvring and sealing its fate - surface craft sunk the crippled battleship less than 13 hours later.

The Swordfish's shortcomings were starkly demonstrated in February 1942 when a strike on German battlecruisers during the Channel Dash resulted in the loss of all attacking aircraft. Immensely vulnerable to enemy fighters, the Swordfish might not have enjoyed such an impressive career if the German Navy had enjoyed the kind of fighter cover that enabled the Japanese to defend their carriers against torpedo attack at Midway. Certainly, the development of new torpedo attack aircraft, saw the



824 Sqn Swordfish in flight over Hong Kong

Swordfish redeployed in an anti-submarine role, equipped with depth-charges or eight 60 lb RP-3 rockets and flying from the smaller escort carriers and MAC ships. Its low stall speed and inherently tough design made it ideal for operation from the MAC carriers in the often severe mid-Atlantic weather, where it was also not at risk from enemy fighters.

Swordfish equipped units accounted for 14 U-boats destroyed.

The Mark II and Mark III variants were both introduced in 1943. The Swordfish Mark II had metal lower wings



Fairey Swordfish II at Duxford Airshow 2002 (© Peter Noble)

to allow the use of rockets, and the Swordfish Mark III added a large centrimetric radar unit. Production ended in 1944 with the Mark IV, which had an enclosed cabin for use by the RCAF, the last operational squadron was disbanded

on 21 May 1945, after the fall of Germany, and the last training squadron was disbanded in the summer of 1946. Almost 2,400 had been built, 692 by Fairey and 1,699 in Sherburn by the Blackburn Aircraft Company, which were sometimes dubbed the 'Blackfish.' The most numerous version was the Mark II, of which 1,080 were made.

The Swordfish received the nickname 'Stringbag' by virtue of the seemingly endless variety of stores and equipment that it was cleared to carry. Crews likened the aircraft to a housewife's string shopping bag, which could adjust to hold any shape or number of packages.

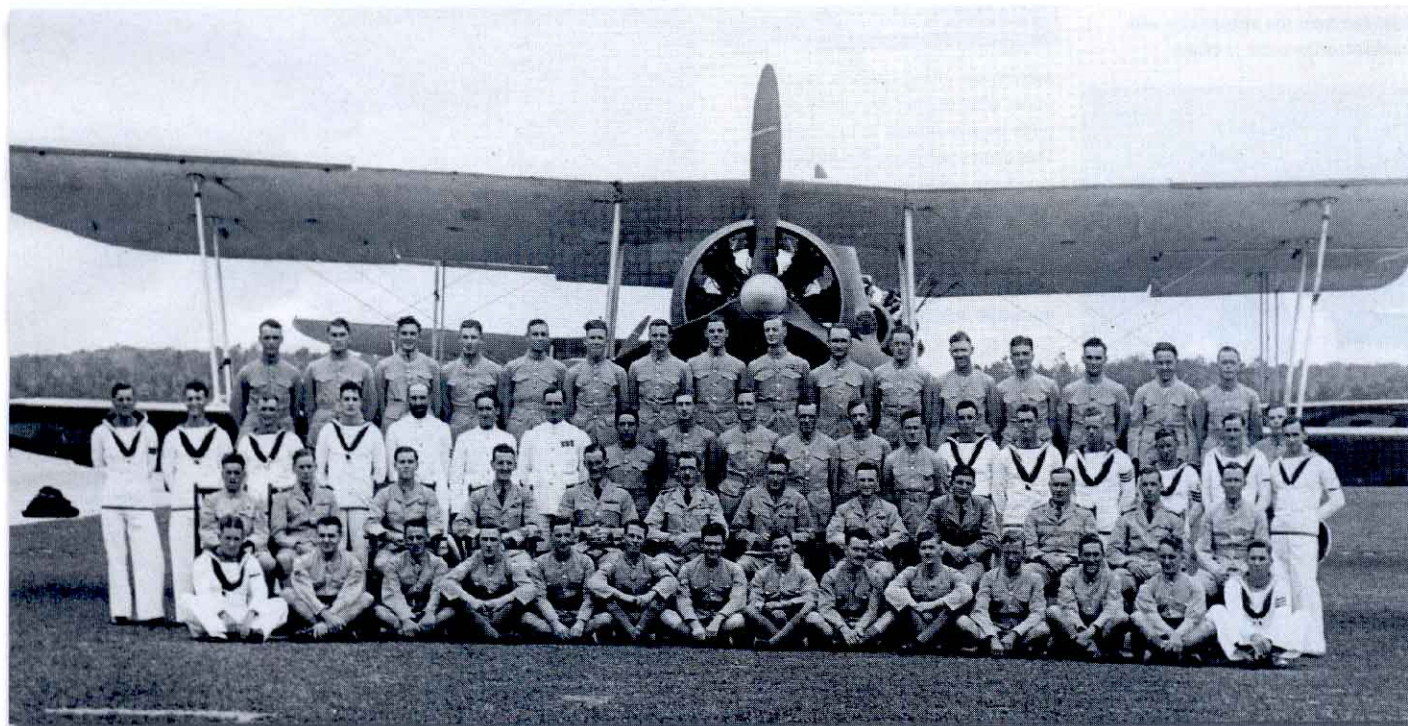
SAMI

Recommended reading:

The Swordfish Story - Ray Sturtivant
 War in a Stringbag - Charles Lamb



Swordfish operating from HMS Ark Royal in 1939



824 Sqn Sworfish and personnel pose for the camera

Modelling the Swordfish in the 21st Century

While the Swordfish may not be everyone's cup of tea, it is one of those aircraft that hangs around the periphery of our knowledge, by virtue of its near legendary wartime career. As far as kits go, it is neither the best nor the worst-served, and for the modeller working in both 1/72 and 1/48 there are certainly options.

Not so forthcoming has been the after-market industry, and small-scale enthusiasts have for many years been denied the opportunity to fill up their Stringbag. Happily, it is inconceivable that in the current frantic climate of kit and accessories production we will have long to wait before such items as smoke floats and 20lb bombs become readily available.

With the emphasis on modelling rather than collecting, then, here is a brief resume of some Swordfish kits that may cross your path.

1/72

Airfix

Airfix's ancient offering has appeared in a number of guises over the years. Both fit and detail show their age, although the Spartan cockpit detail is no worse than others in this scale. Airwaves have produced an etched set for the Airfix kit, which has recently been re-issued by Hannants. This addresses some of the problems, but most of the parts will be applicable to other kits as well.

Matchbox

Despite the dreadful panel lines and crudely moulded details, the Matchbox kit is probably the most buildable of the 1/72 kits. Revell's recent re-issue features a superb decal sheet, and while a lot of scratchbuilding will be necessary to produce a good replica, the basic moulding offers less compromises than most.

FROG

FROG's tooling has been re-issued by many companies over the last decade or so. It has some excellent exterior details, but is let down by the pilot's windscreen which is a heavy moulding that includes part of the forward fuselage. This makes it difficult to thin down – unlike the Matchbox part, which is a simple affair that can be sanded to scale and polished, and will increase the amount of scratch building required.

This aside, the kit is worth the effort, and is also the only version in 1/72 with floats. MPM have boxed the kit a couple of times, with both resin and etched interior details, while Novo, Eastern Express, and no doubt others have had



a crack at the old moulds too.

All of the currently available kits have their shortcomings, and there is no easy route to producing an acceptable Swordfish in this scale – which is a shame, as the aircraft deserves a lot more attention.

1/48

Merit

Most recently boxed by Smer, this ancient rendering features the moulded on decals, and the lack of detail one would expect from a kit older than MAM Editor Neil Robinson, but is not entirely devoid of merit. Modellers unprepared to shell out for the Tamiya kit will take comfort from the knowledge that with a bit of work, and a lot of scratchbuilding, a decent result can be obtained.

MDC

Some years ago MDC produced an all-resin kit, which many regard as superior to the Tamiya issue. This kit is not currently listed as available, but a selection of detail and conversion parts can be viewed at the manufacturer's website: www.shopcreator.com/mail/modeldesignconstruction

Tamiya

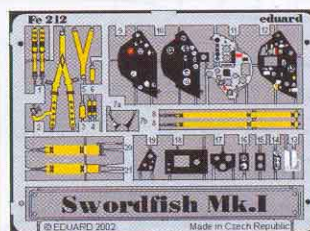
Tamiya's superb kit has just come out in its fourth version. Previous issues have seen a Mk.I, a Floatplane, a Mk. I with a transparent fuselage, and now the Mk.II. Given the success of the kit, and its undoubted popularity, a Mk.III may be on the agenda at some point, but how about scaling it down to 1/72?



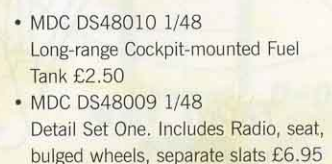
Accessories

With the exception of Airwaves etched set, accessories focus on the 1/48 Tamiya kit.

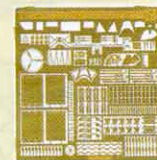
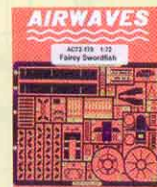
- Aeroclub BV082 1/48 Metal Swordfish undercarriage, arrest hook, wheels and gun £4.50



- Airwaves AEC72178 1/72 Etched parts for Airfix kit £5.99
- Czech Master Kits CMK4062 1/48 Floatplane conversion £16.99
- Czech Master Kits CMK4063 1/48 Swordfish Mk III conversion £13.60
- Czech Master Kits CMK4163 1/48 Separate control surfaces £7.85
- Eduard ED48308 1/48 Etched parts Mk I £10.50
- Eduard ED48362 1/48 Etched parts Mk I float plane £10.50
- Eduard ED49212 1/48 Etched parts Mk I including pre-painted £11.80
- Eduard EDFE212 1/48 Etched parts Mk I pre-painted £4.99



- MDC DS48010 1/48 Long-range Cockpit-mounted Fuel Tank £2.50
- MDC DS48009 1/48 Detail Set One. Includes Radio, seat, bulged wheels, separate slats £6.95
- MDC CV48040 1/48 Mk III conversion. Includes bottom wing, cockpit coaming, oil cooler, radar housing and receiver, exhaust extension £7.50
- MDC CV48038 1/48 Floatplane conversion for the Tamiya kit. Includes Floats, struts, Rudders and replacement rear fuselage £10.50
- MDC CV48039 1/48 Mk II conversion. Includes bottom wing and oil cooler, Price £5.25
- MDC MDC48001 Fairey Swordfish Etched Brass £14.50



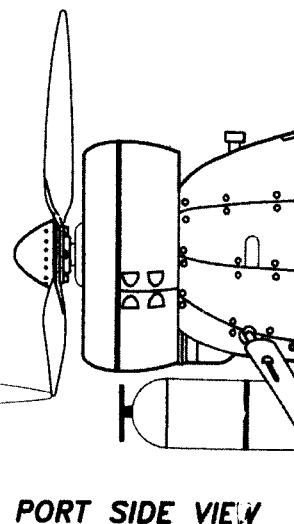
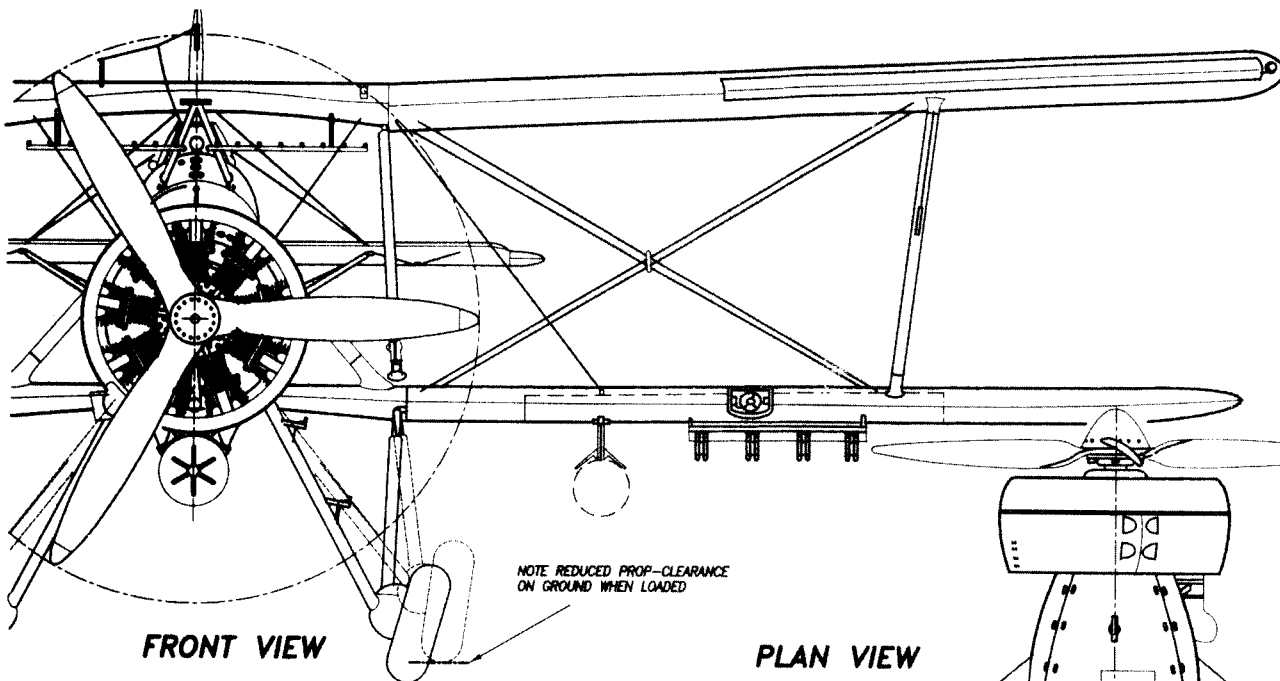
- Quickboost #48088 1/48 Fairey Swordfish Mk I exhaust £1.99
- Tamiya #61069 1/48 Swordfish strut bracing set £6.99

Decals

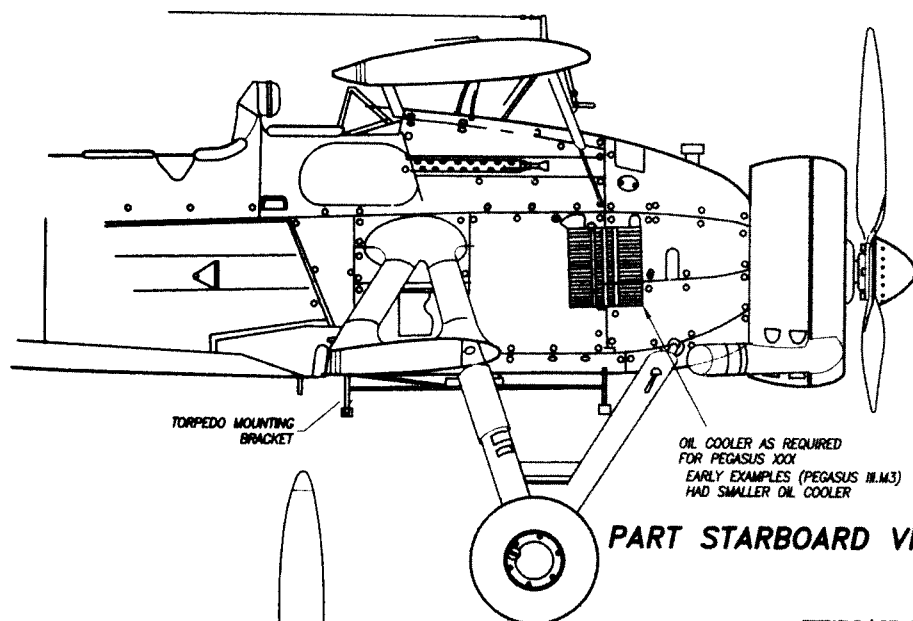
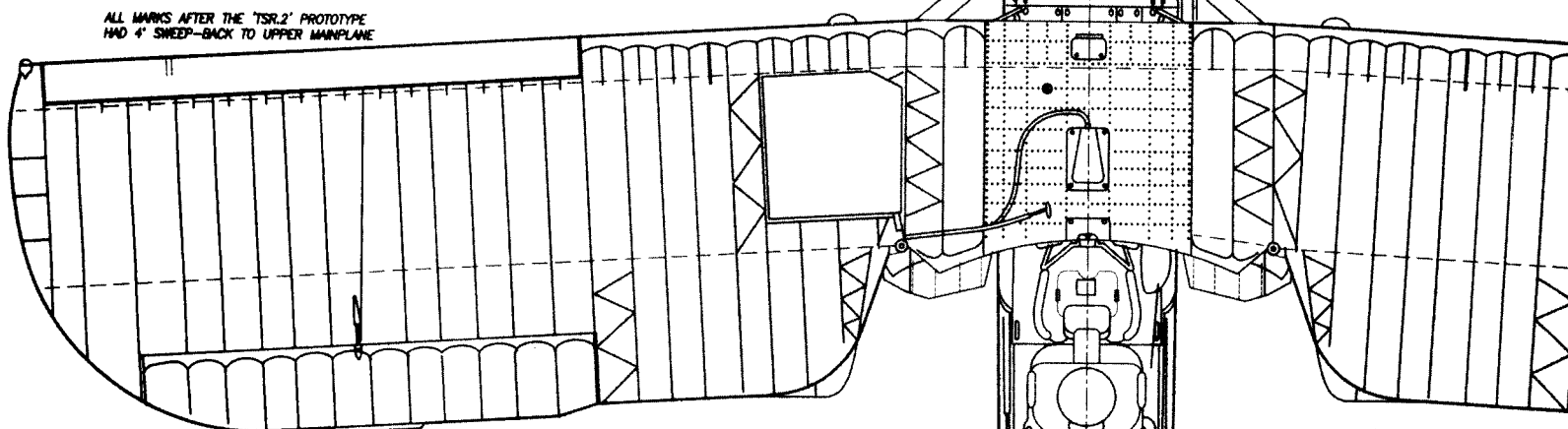
The following include Swordfish, although only the Xtradecal sheet is entirely devoted to the type.

- AeroMaster #72192 1/72 Fleet Air Arm Pt 1 £5.50
- AeroMaster #72193 1/72 Fleet Air Arm Part 2 £5.50
- Dutch Decal #48025 1/48 Dutch Assortment. £4.99
- Xtradecal X03948 1/48 Swordfish Mk II/II £5.50

SAMI



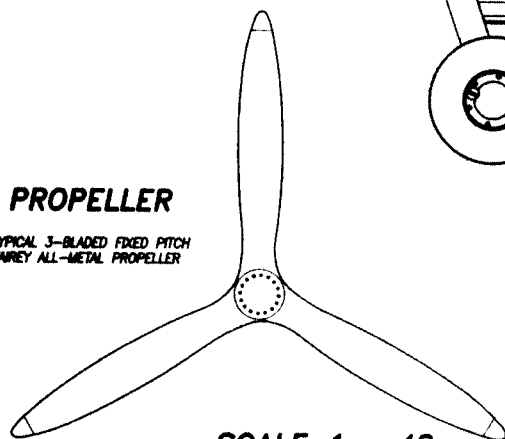
ALL MARKS AFTER THE 'TSR.2' PROTOTYPE HAD 4° SWEEP-BACK TO UPPER MAINPLANE



EFFECTIVE 'LEFT RUDDER' TO COUNTERACT TORQUE EFFECT

PROPELLER

TYPICAL 3-BLADED FIXED PITCH FAIRY ALL-METAL PROPELLER



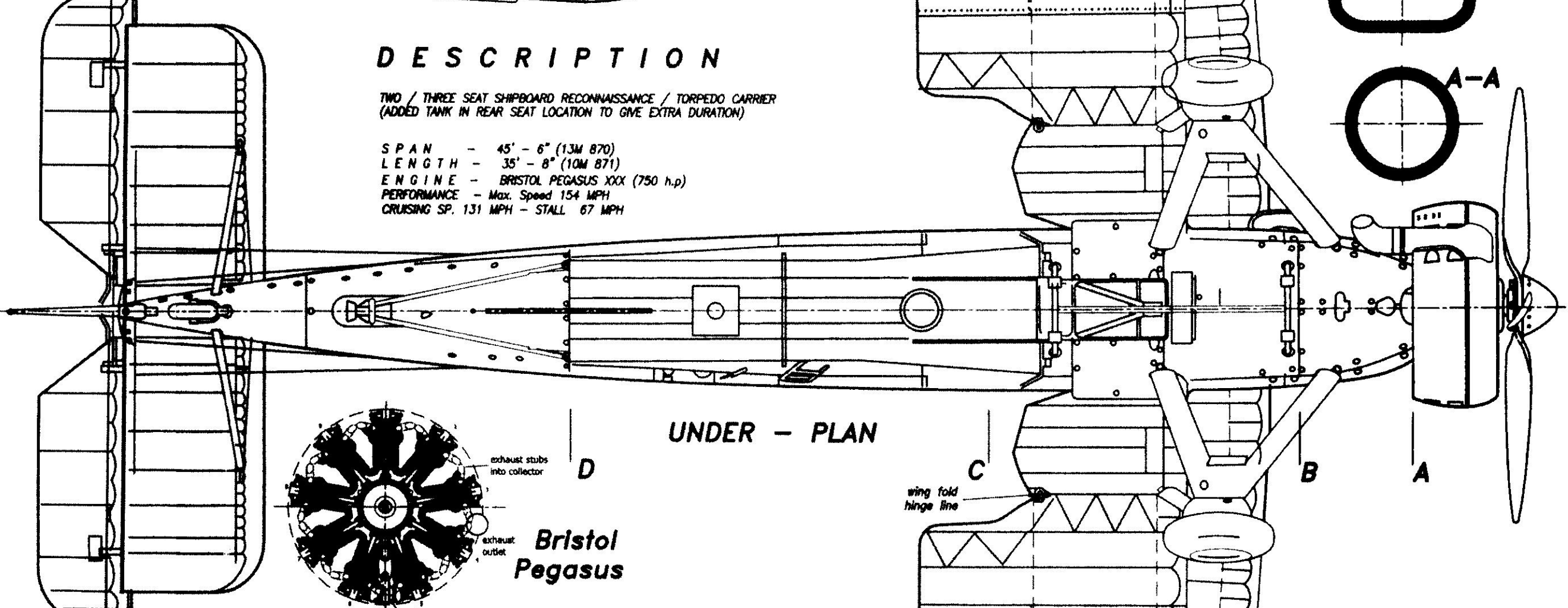
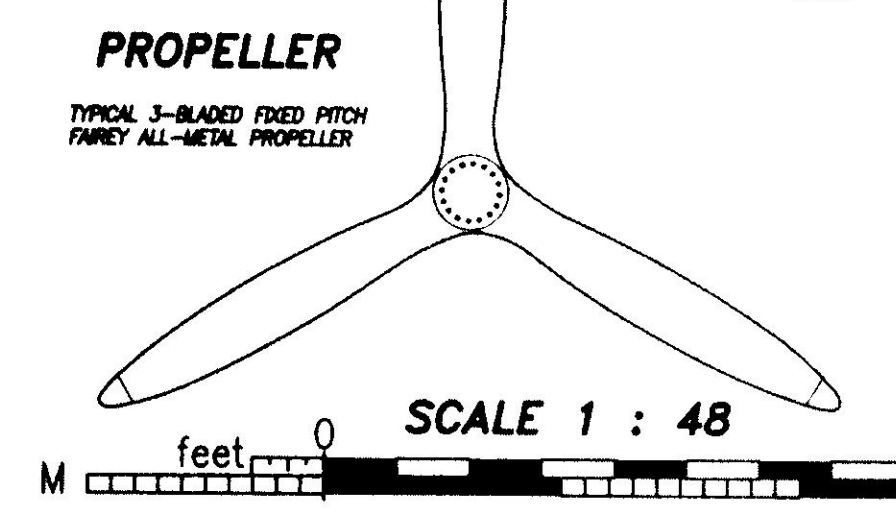
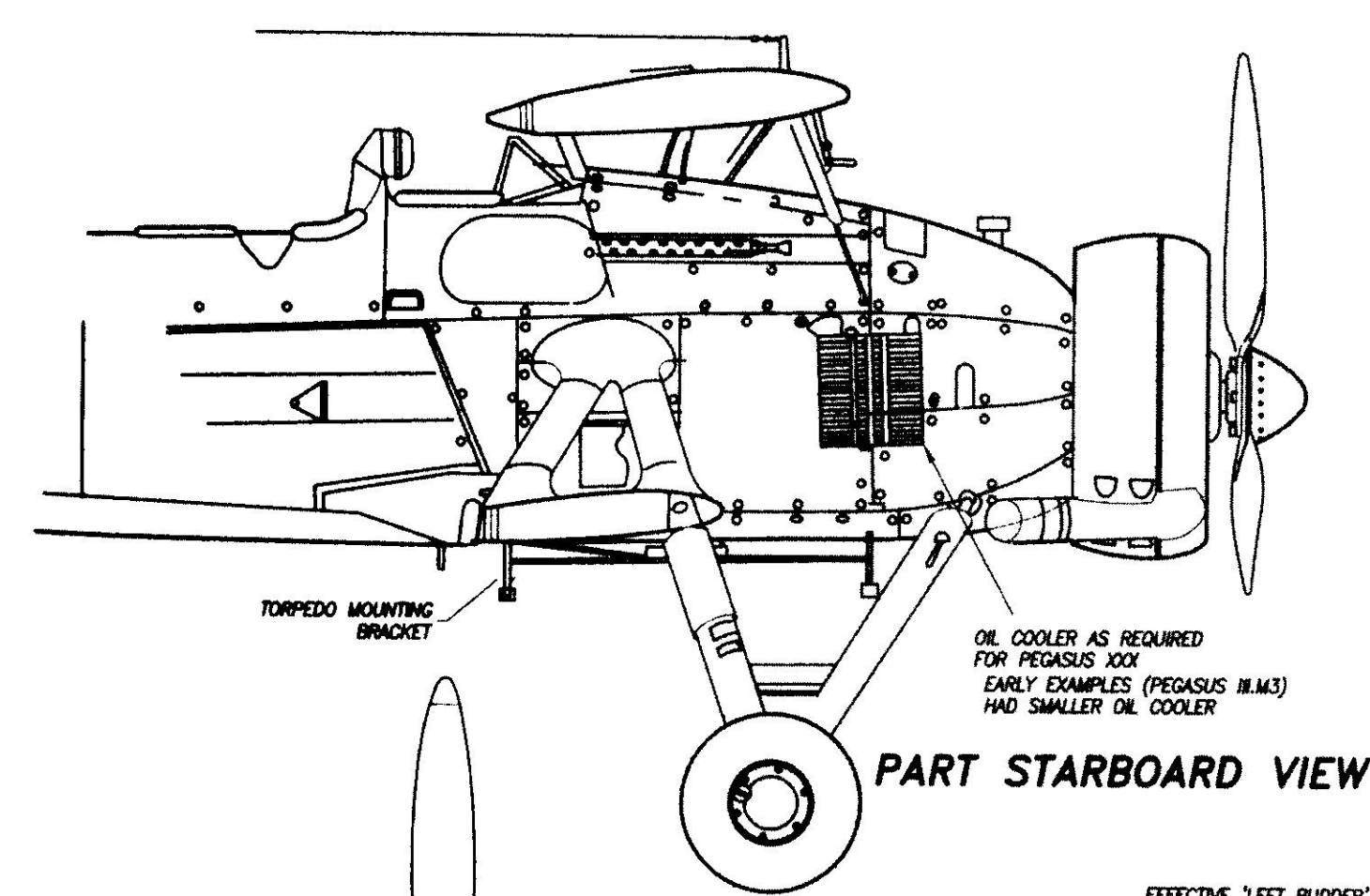
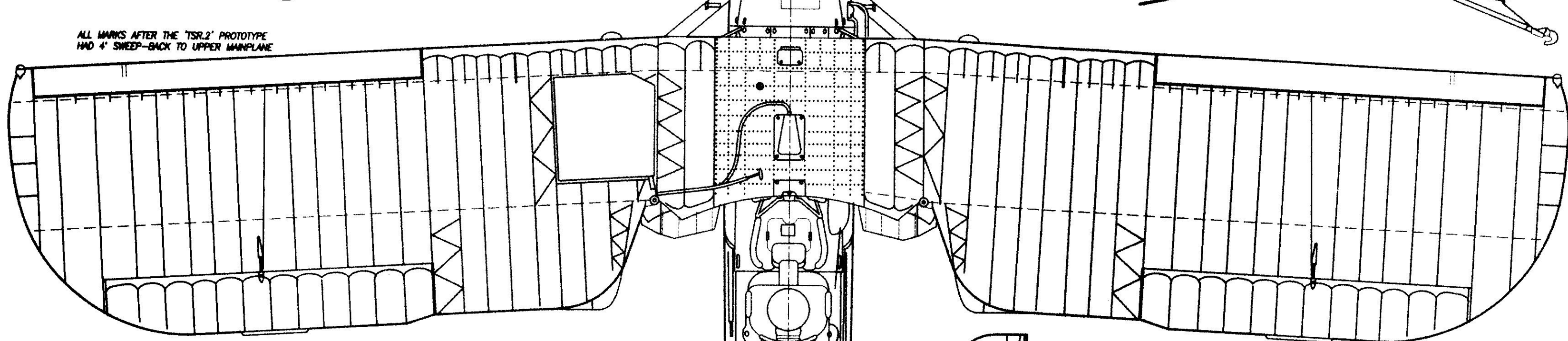
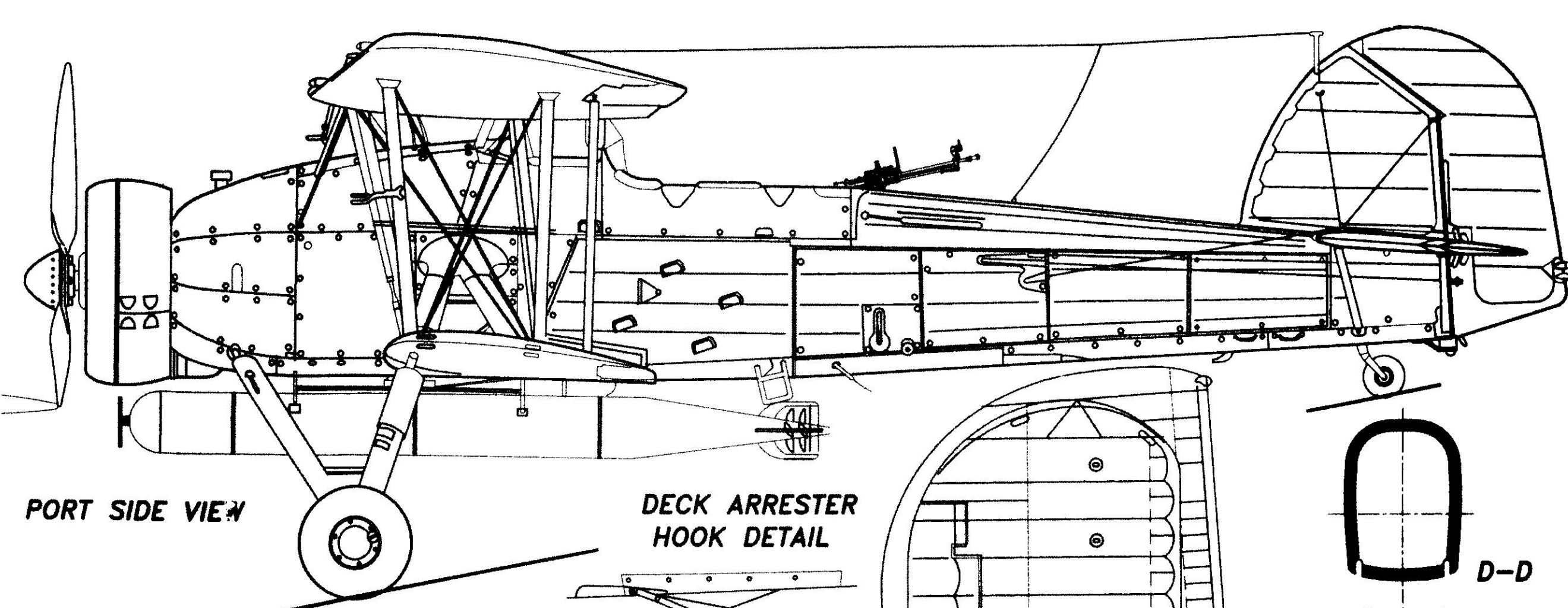
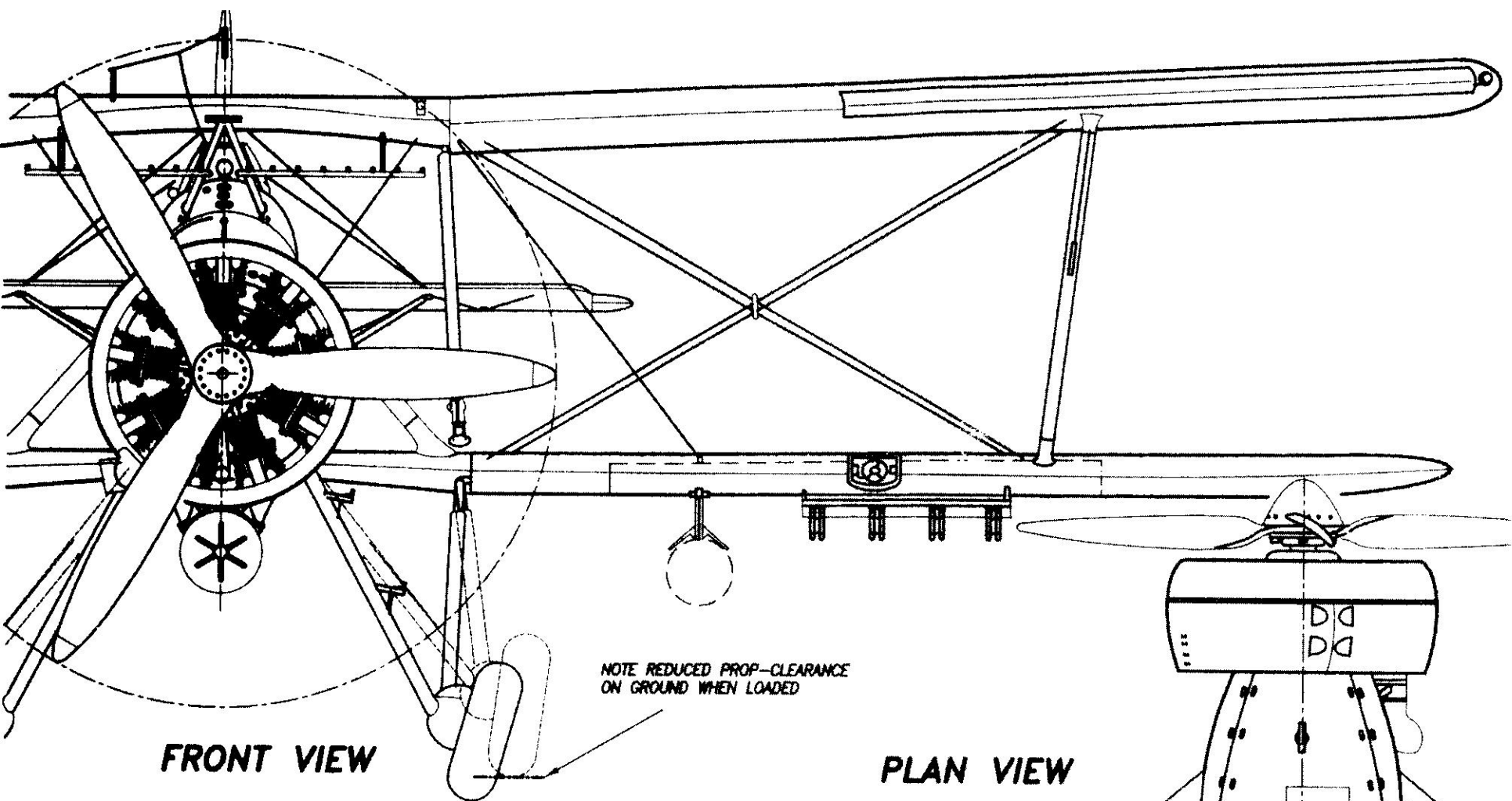
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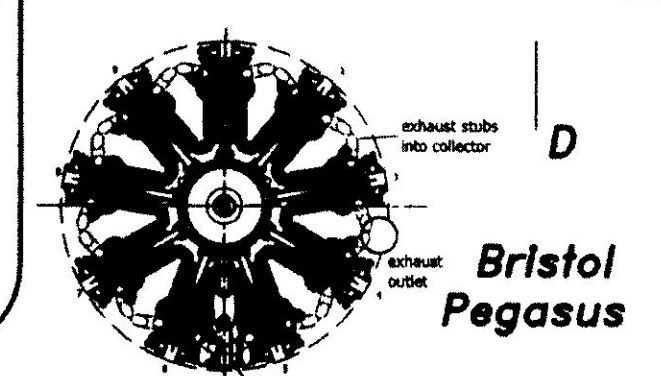
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DESCRIPTION

TWO / THREE SEAT SHIPBOARD RECONNAISSANCE / TORPEDO CARRIER
(ADDED TANK IN REAR SEAT LOCATION TO GIVE EXTRA DURATION)

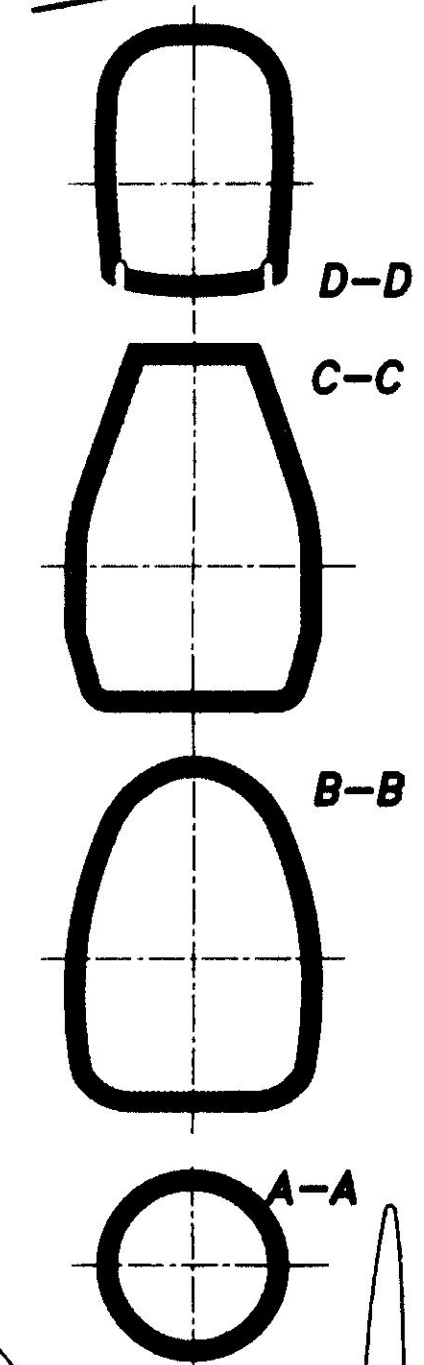
SPAN - 45' - 6" (13M 870)
LENGTH - 35' - 8" (10M 871)
ENGINE - BRISTOL PEGASUS XXX (750 h.p.)
PERFORMANCE - Max. Speed 154 MPH
CRUISING SP. 131 MPH - STALL 67 MPH



FAIREY SWORDFISH Mk.2

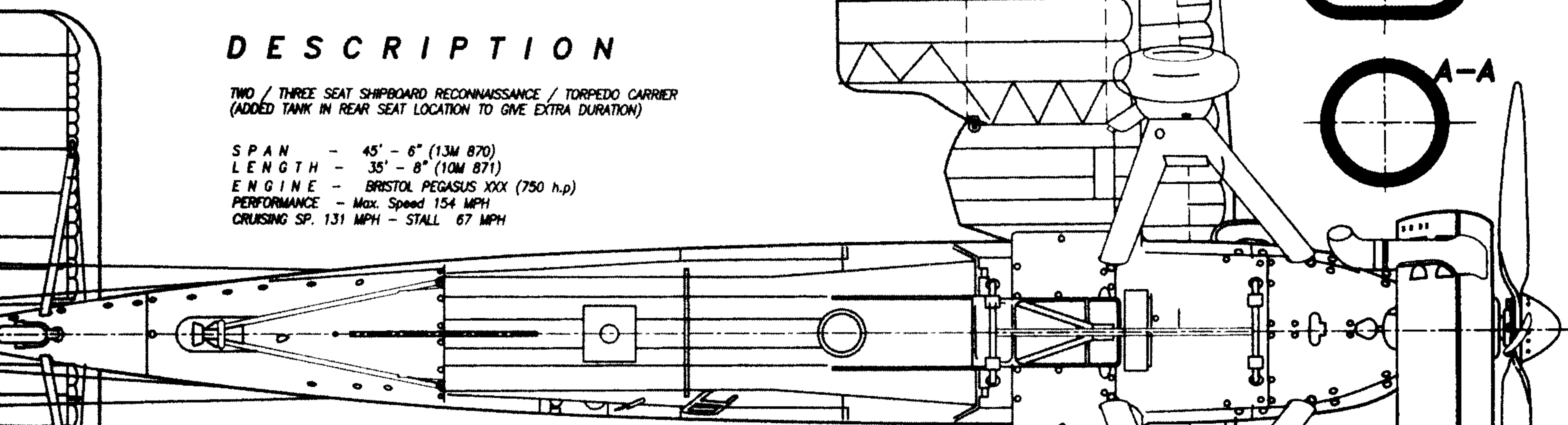
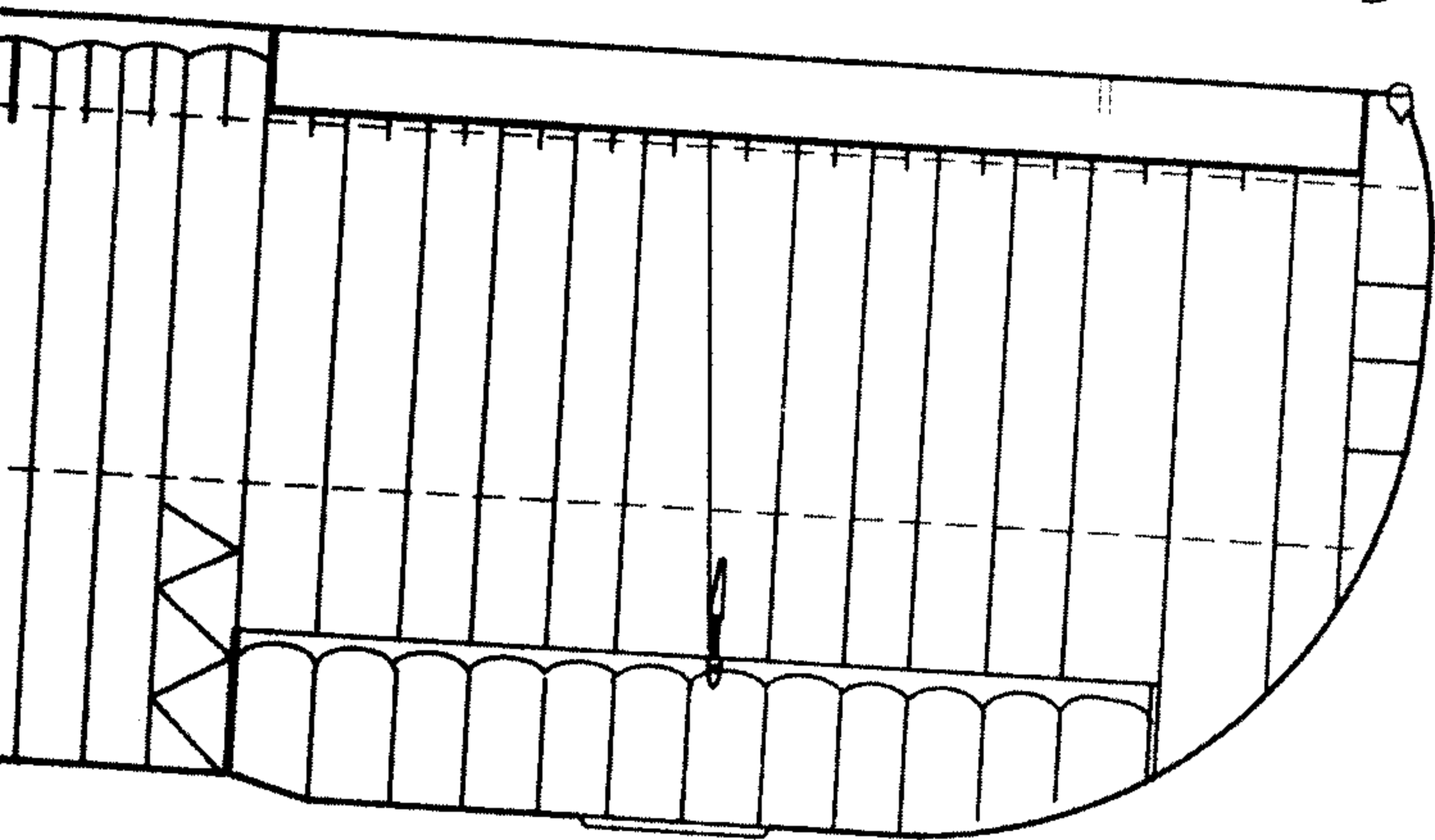
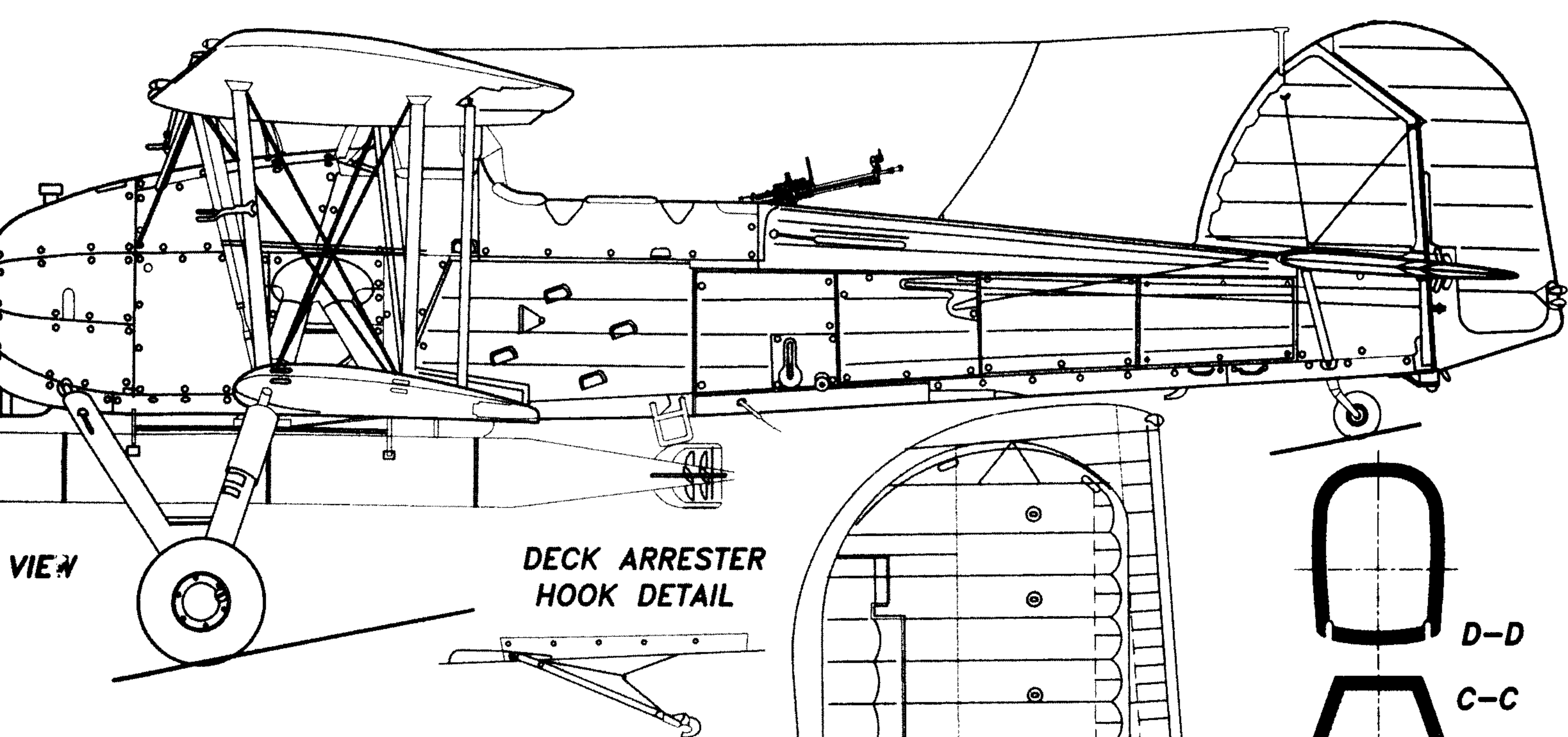
HISTORY

ALREADY OUTDATED WHEN INTRODUCED TO OPERATIONAL UNITS IN 1937, THE SWORDFISH CONTINUED IN USE UNTIL THE END OF WW2 WHEN TYPES INTRODUCED TO SUPERSEDE IT HAD THEMSELVES BEEN DISCONTINUED. ARMED WITH A-S ROCKETS, THE Mk.III REPLACED THE TORPEDO MOUNTING WITH A BULBOUS HOUSING BETWEEN UNDERCARRIAGE LEGS CONTAINING A.S.V. (AIR TO SURFACE VESSEL) RADAR WHICH TOGETHER WITH THE LEIGH LIGHT ENDED THE REIGN OF THE U-BOATS.



METAL SKINNED UNDERSIDE TO LOWER MAINPLANES INTRODUCED WHEN ROCKET RACKS WERE FIRST INTRODUCED OPERATIONALLY

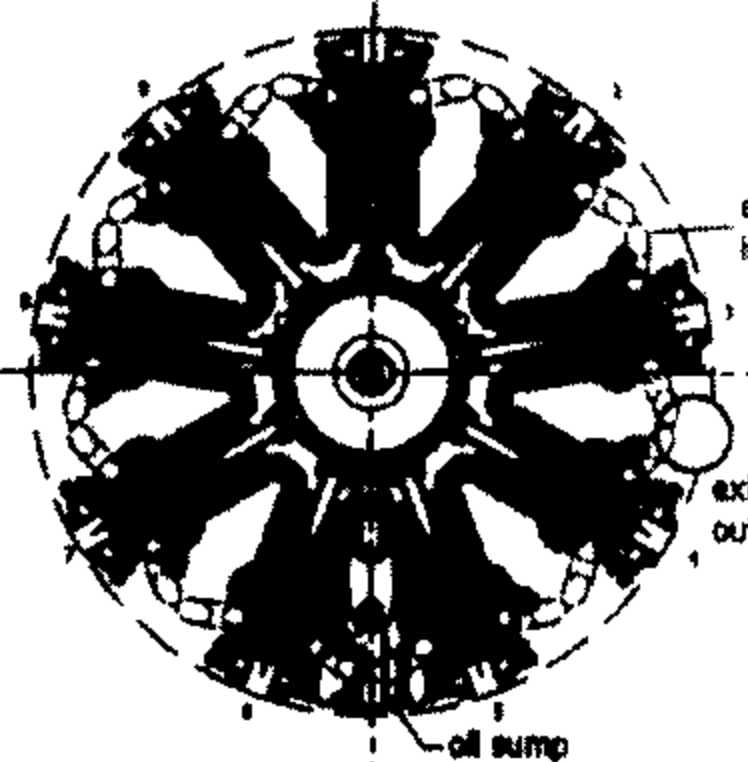
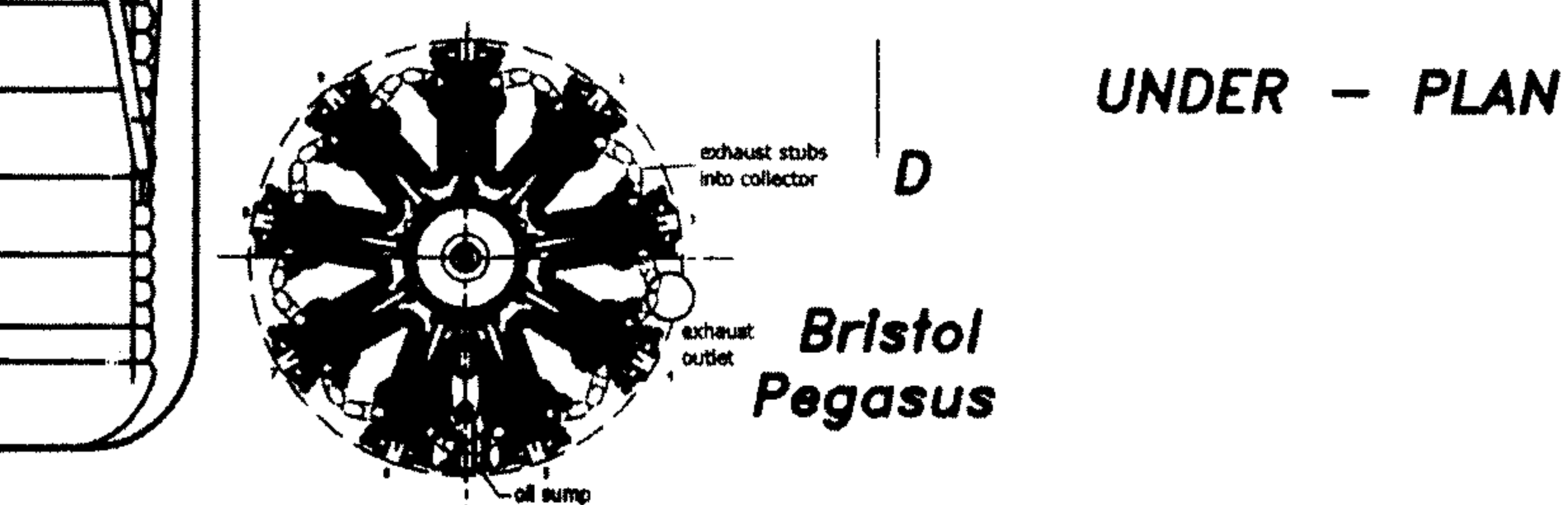
Len Whalley 2007
aeroplans.co.uk



DESCRIPTION

TWO / THREE SEAT SHIPBOARD RECONNAISSANCE / TORPEDO CARRIER
(ADDED TANK IN REAR SEAT LOCATION TO GIVE EXTRA DURATION)

SPAN - 45' - 6" (13M 870)
LENGTH - 35' - 8" (10M 871)
ENGINE - BRISTOL PEGASUS XXX (750 h.p.)
PERFORMANCE - Max. Speed 154 MPH
CRUISING SP. 131 MPH - STALL 67 MPH



FAIREY SWORDFISH Mk.2

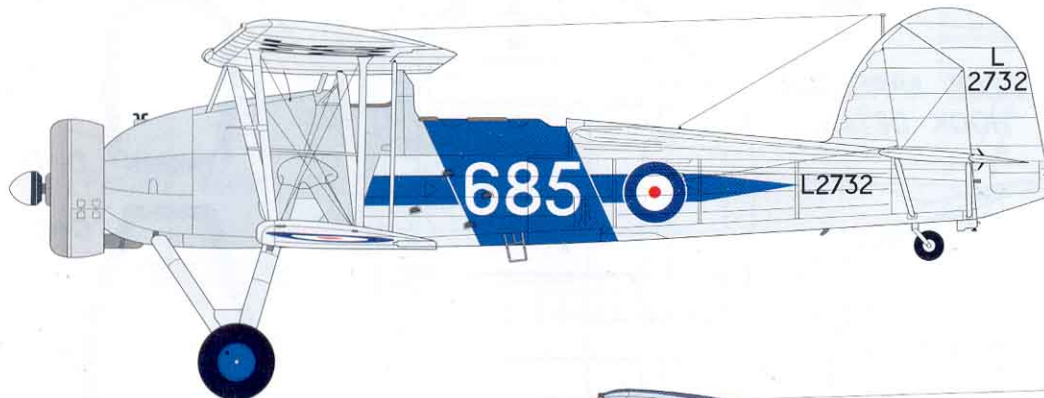
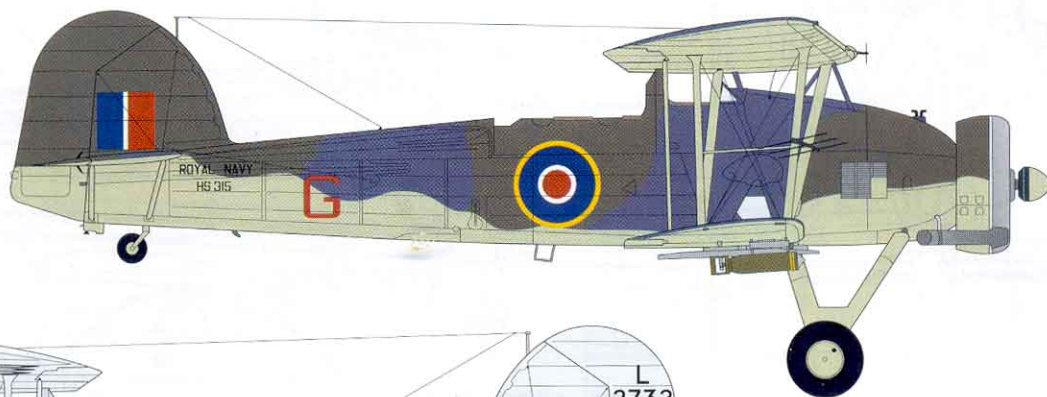
HISTORY

ALREADY OUTDATED WHEN INTRODUCED TO OPERATIONAL UNITS IN 1937, THE SWORDFISH CONTINUED IN USE UNTIL THE END OF WWII WHEN TYPES INTRODUCED TO SUPERSEDE IT HAD THEMSELVES BEEN DISCONTINUED. ARMED WITH A-S ROCKETS, THE Mk.IV REPLACED THE TORPEDO MOUNTING WITH A BULBOUS HOUSING BETWEEN UNDERCARRIAGE LEGS CONTAINING A.S.V (AIR to SURFACE VESSEL) RADAR WHICH TOGETHER WITH THE LEIGH LIGHT ENDED THE REIGN OF THE U-BOATS.

METAL SKINNED UNDERSIDE TO LOWER MAINPLANES INTRODUCED WHEN ROCKET RACKS WERE FIRST INTRODUCED OPERATIONALLY

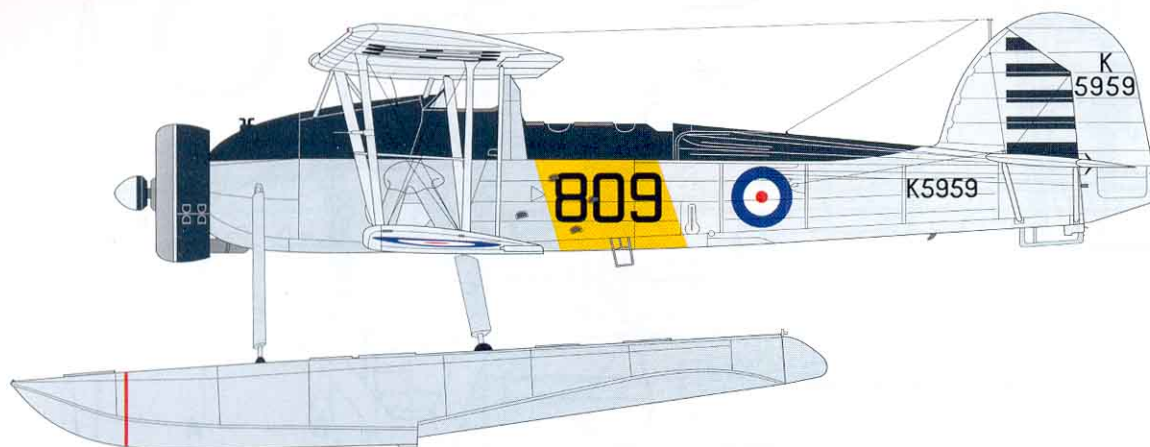
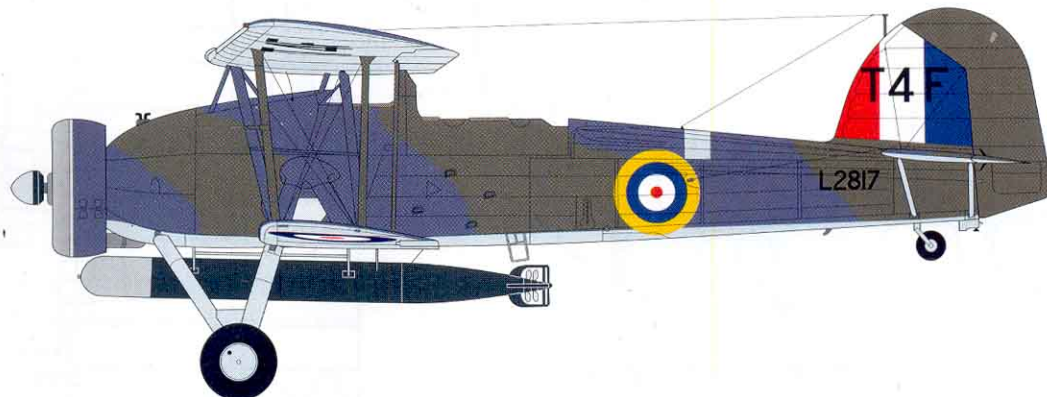
Len Whalley 2007
aeroplans.co.uk

HS315, Swordfish Mk II, G of No.842 NAS, FAA, HMS Fencer, March to August 1943. Extra Dark Sea Grey/Dark Slate Grey /Sky. Roundels: Fuselage - 36" type C1, upper wing - 52" type B. Fin flash 24x24".



L2732, Swordfish Mk I, 685 of No.821 Naval Air Squadron, Fleet Air Arm, on board HMS Courageous, January to June 1938. Roundels: Fuselage - 25 inch; Wings - 52" red/white/blue. Overall Aluminium dope except for metal panels in Cerrux Grey and cowling rings probably anodised metal.

L2817, Swordfish Mk I, T4F of No.767 NAS, FAA, Hal Far, Malta in October 1939. Extra Dark Sea Grey/Dark Slate Grey /Sky Grey. Roundels: Fuselage - 35" Type A1, under wing - 52" type A, upper wing - 52" Type B. Fin flash in equal divisions.



K5959, Swordfish Mk I, 809 of No.823 Naval Air Squadron, Fleet Air Arm, on board HMS Glorious, May/June 1936. Roundels: Fuselage - 25 inch; Wings - 52" red/white/blue. Overall Aluminium dope except for metal panels in Cerrux Grey. Black top decking, cowling and fin bars. Floats and supports in Cerrux Grey.

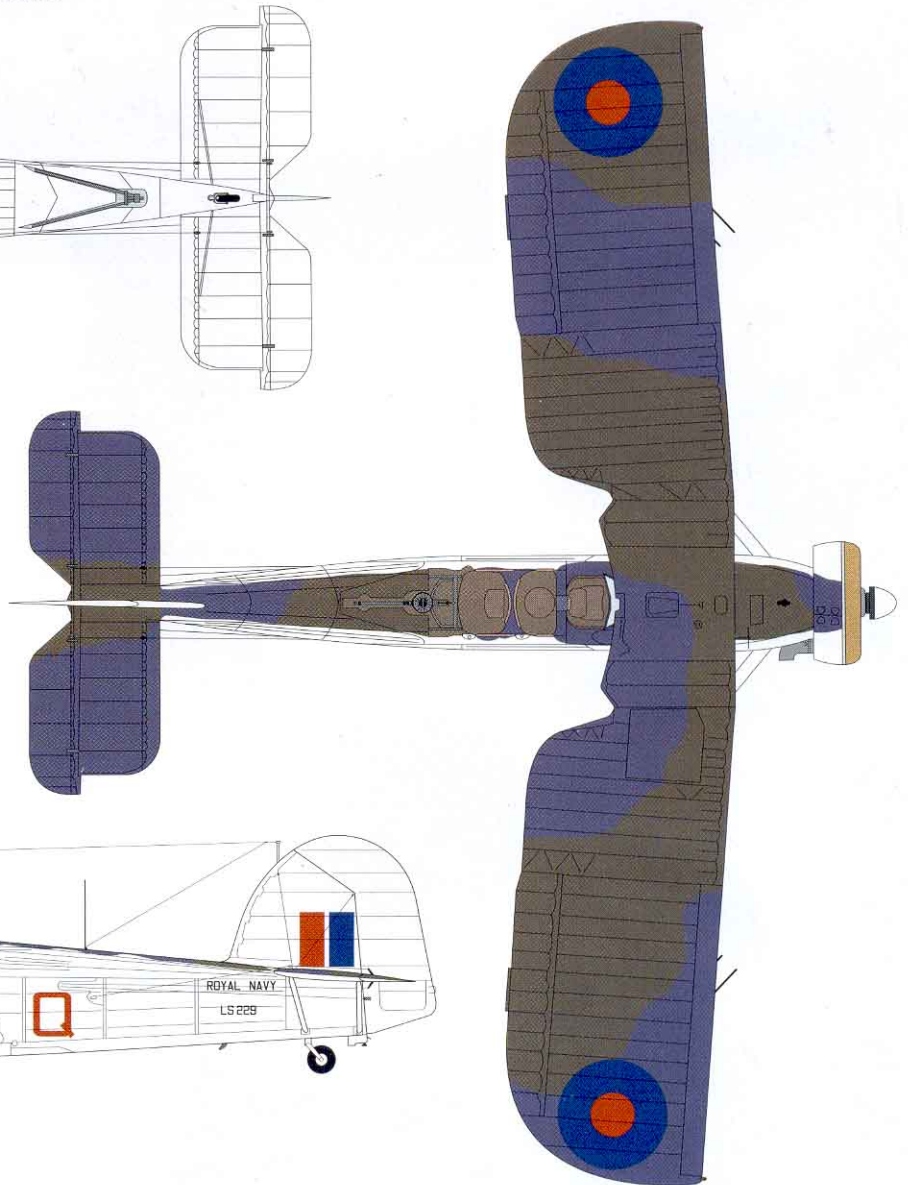
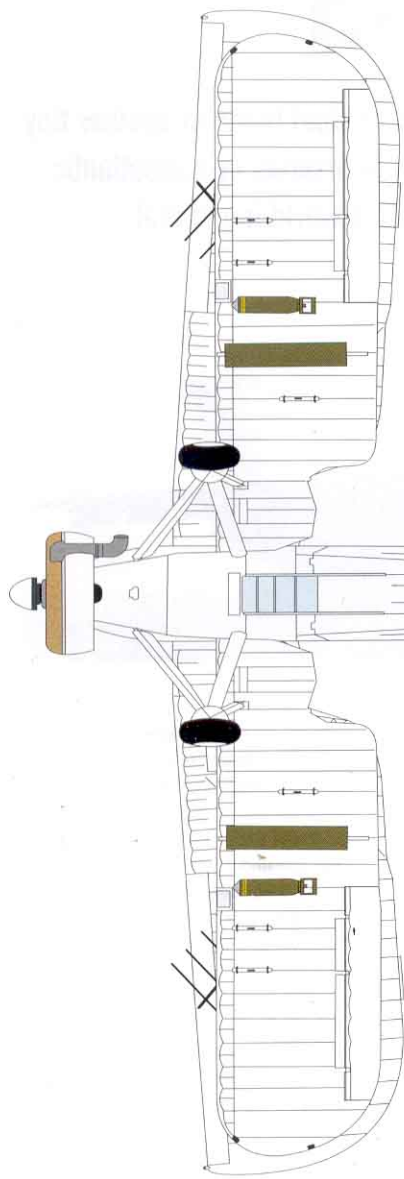
LS274, Swordfish Mk II, 1F of No.818 NAS, FAA, HMS Unicorn, East Indies Fleet, April 1944. Extra Dark Sea Grey/Dark Slate Grey /White. Roundels: 36" type C1, upper wing - 52" type B modified by replacing the red centres with white. The 24x24" fin flash was also modified.



Colour Art © 2007 David Howley



LS229, Swordfish Mk II, M1Q of Q Flight, No.836 NAS, FAA, based at Maydown and Dartmouth, NS, Canada for operations on MAC Ships in 1944. Extra Dark Sea Grey/Dark Slate Grey /White. Roundels: Fuselage - 36" type C1, upper wing - 52" type B. Fin flash 24x24".



Colour Art © 2007 David Howley

Swordfish Colour Chart



All photos are courtesy of Lockheed-Martin

Chief Test Pilot Leo Sullivan smiles
for the cameras after the successful
first flight

Flight of the C-5

For those who need a break from squinting down the scalpel blade at another tiny piece of plastic, our veteran columnist takes the first in a series of Transatlantic looks at the world of modeling, aircraft, and the aviation world in general



The C-5 prototype takes to the air for the first time

At the risk of being accused of pandering to our esteemed Editor, I thought it would be fun to kick this column off with an anecdote about one of his favorite aircraft, the Lockheed-Martin C-5, hereafter referred to as the Lockheed C-5.

At the time the C-5 was being built, I recall that there were, at one point, nearly 30,000 employees at the mammoth Lockheed complex in the Atlanta suburb of Marietta. Part of Building B-1, the main assembly shed,

dating back to Bell B-29 days, was used to assemble the fuselage and other components. The wings were manufactured by Avco, which got its start in an old Vultee Aircraft plant in Nashville, Tennessee, and then shipped on special rail cars to Marietta. These were attached in a huge, six-story hangar that held four C-5s. The tail assembly was attached in another purpose-built structure nicknamed 'the cathouse.'

At the time, Building B-1, and most of the Lockheed real estate, was still owned by the U.S. Air Force (which had built the facility as a B-29 'shadow plant') and leased to Lockheed as it had been since 1951. Lockheed took over the plant that year, and put many former Bell Aircraft employees to work refurbishing B-29s for the Korean War.

When the C-5A was rolled out on March 2nd 1968, President Lyndon Baines Johnson thought it was worth the trouble to attend the event. How often does that happen these days? There has been so much political hay made over expensive (and presumed to be unnecessary) defense projects in this country that one wonders if such events are ever attended by the President any more.

Before and especially after the prototype rollout, we began to hear a lot of unexplained noise at home, day and night, coming from the general direction of Lockheed. I think what we were hearing was engine testing, and perhaps the engines for the first flight being run in. Nothing like the C-5 had been built at that time. The Boeing 747 was still practically a year off, and besides, comparing the 747 to the C-5 was, and is, like comparing apples and oranges. The 747 might fly faster but it damn sure couldn't be landed on an ice runway at McMurdo Station, Antarctica.

And the 747 would never be considered a National Asset like each C-5. (Don't get me wrong, I think the B747 is a beautiful aircraft, especially the 747SP.)

It was the last week of June 1968, and just under four months after the Galaxy had rolled out in front of the President, and ground testing of the Galaxy was underway. On the last Thursday and Friday in June, taxi tests merited major coverage in the *Atlanta Journal-Constitution*. These tests were apparently going well, and the question in Atlantian's minds was, 'would the C-5 fly that weekend, and how well would it fly?'

My mother, a Registered Nurse and 40-year veteran at Lockheed, was asked to work Saturday at the Lockheed Flight Line. Like everyone at the plant, she was confident the airplane would fly and fly well, but the reality was that if something happened to the C-5 that Saturday, Lockheed wanted plenty of Medical Department personnel on site. After all, this was before the days of computer-aided design.

She spent most of Saturday riding in the back seat of a red chase car that followed the C-5 up and down the runway. This Ford Galaxy ambulance was being driven by an emergency medical technician (EMT). Another EMT was riding in the front and my mom was riding in the back seat with one of the Lockheed doctors (an Air Force veteran himself).

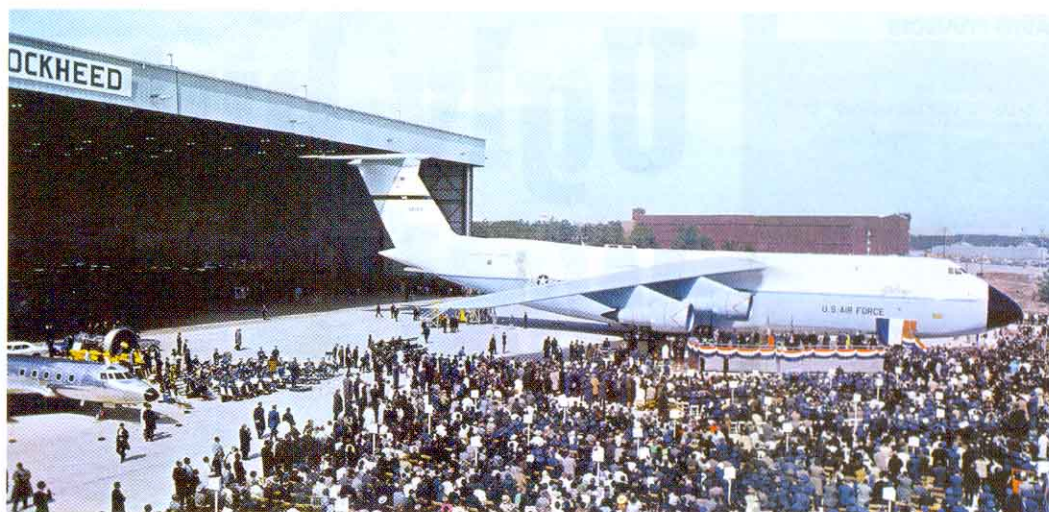
The C-5 did not fly that day. My mother related the day's events at supper that evening and said that she thought that Leo Sullivan was going to take off, but that several tires had blown out on the main landing gear at one point, which spooked everybody. The tires were replaced and the flight postponed. (Readers of the *Lockheed Southern Star* newspaper would later



learn of an incident that day in which an engineer's Volkswagen Beetle, left out of gear and sans parking brake, was harmlessly moved a considerable distance by the C-5's jet wash, much to the amusement of fellow employees).

The next morning, June 30th, we all got up and ate our usual Sunday breakfast of pancakes and bacon. My dad turned on his little RCA transistor radio and tuned it to the clear-channel AM station, *WSB*. There was already live coverage from Lockheed and it was clear from the sound emanating from the tinny speaker that the Chief Test Pilot was not going to fool around that morning. As we ate, there were several high-speed taxi runs in which Leo Sullivan lifted the ship's nose gear completely clear of the runway. On the last takeoff run and without any hesitation, Sullivan simply accelerated down the runway, gently rotated the nose and held it. The C-5 was airborne!

Our plans to go to church were summarily changed and we all threw on some cool clothing and hurried out to an area of Marietta that we hoped would put us in a good position to watch the plane land, although we surmised that the C-5 would only be in the air for an hour or so. Obviously, the airplane would not be flown over a populated area and we were sure that it had headed North and was out in the sticks somewhere between Atlanta and Chattanooga,



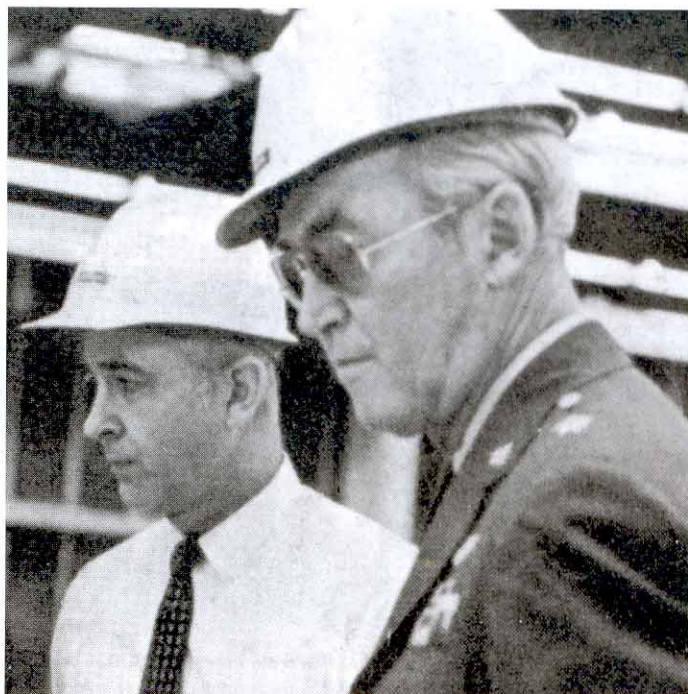
President Johnson and a large crowd were ready for the C-5 roll-out

Tennessee.

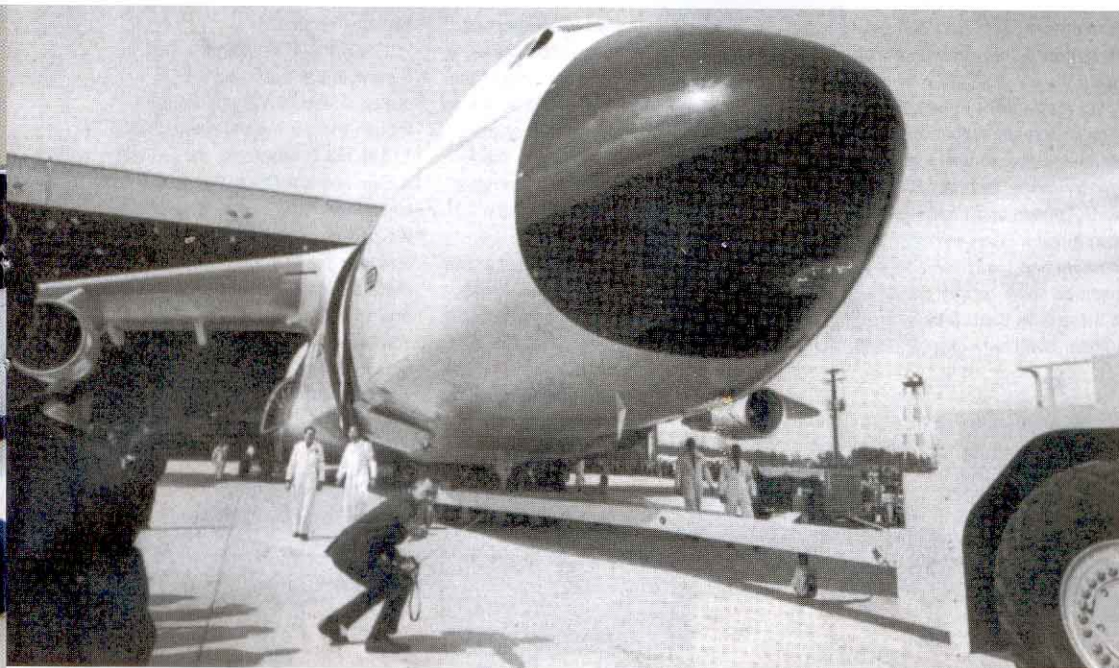
We pulled into a parking lot that was situated on a hill only a half mile from the arrival end of the runway and waited. It wasn't long before the news came over *WSB* that the C-5 was on a long final for the runway. This meant that it would pass within a couple of miles of our house, but we would have an even better view of its approach from our vantage point.

Other cars quickly arrived until every parking lot a quarter mile South and North of the runway threshold started to fill with vehicles. Everyone immediately got out and peered to the East. We spotted the aircraft about 8 miles out and its shape was instantly recognizable. I recall being struck with the impression that the C-5 was not far out, yet it did not seem to be getting bigger as I thought it would. It appeared to stay the same size for a long time, but when it got to within a couple of miles of us, everyone's jaw dropped.

All of us had seen pictures and newsreels of the airplane, but the visual impression of the real thing was now



Then-Brigadier General Jimmy Stewart visited Lockheed just prior to the rollout and later flew a C-5



The manager of the C-5 Pre-Final and Final Assembly Department personally signals the tug driver during rollout.

overwhelming. As it got closer and closer, we could hear the distinctive howl of the engines. All of the hair on my arms and neck was standing up. As the aircraft passed over us at about 150 feet of altitude, it literally took up the entire sky and blocked out the sun. What a day to forget my camera!

We watched, transfixed, as the ship smoothly flared over the Dobbins runway just a half mile away from us and executed a perfect landing. There was a wave of applause and cheers from the crowd as C-5 Ship 001 rolled out of its landing.

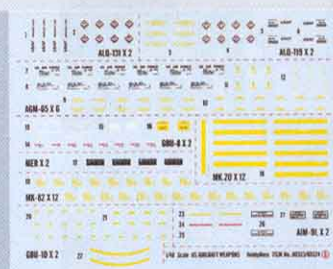
Today, I work just a short distance from Charleston AFB, South Carolina. This is a major base for the C-17, but I always can tell when a C-5 is in the pattern. I can't help looking out the window for it, either.

SAM



N/AW A-10 Thunderbolt II

Scale: 1/48 Kit No: 80324
Price: £25.99 Decal Options: 1
Panel Lines: Recessed Status: New Tooling
Type: Injection Moulded Plastic
Parts: Plastic 108, Clear 18, Rubber Tyres 3
Manufacturer: HobbyBoss
UK Importer: Creative Models



A-10 Thunderbolt II

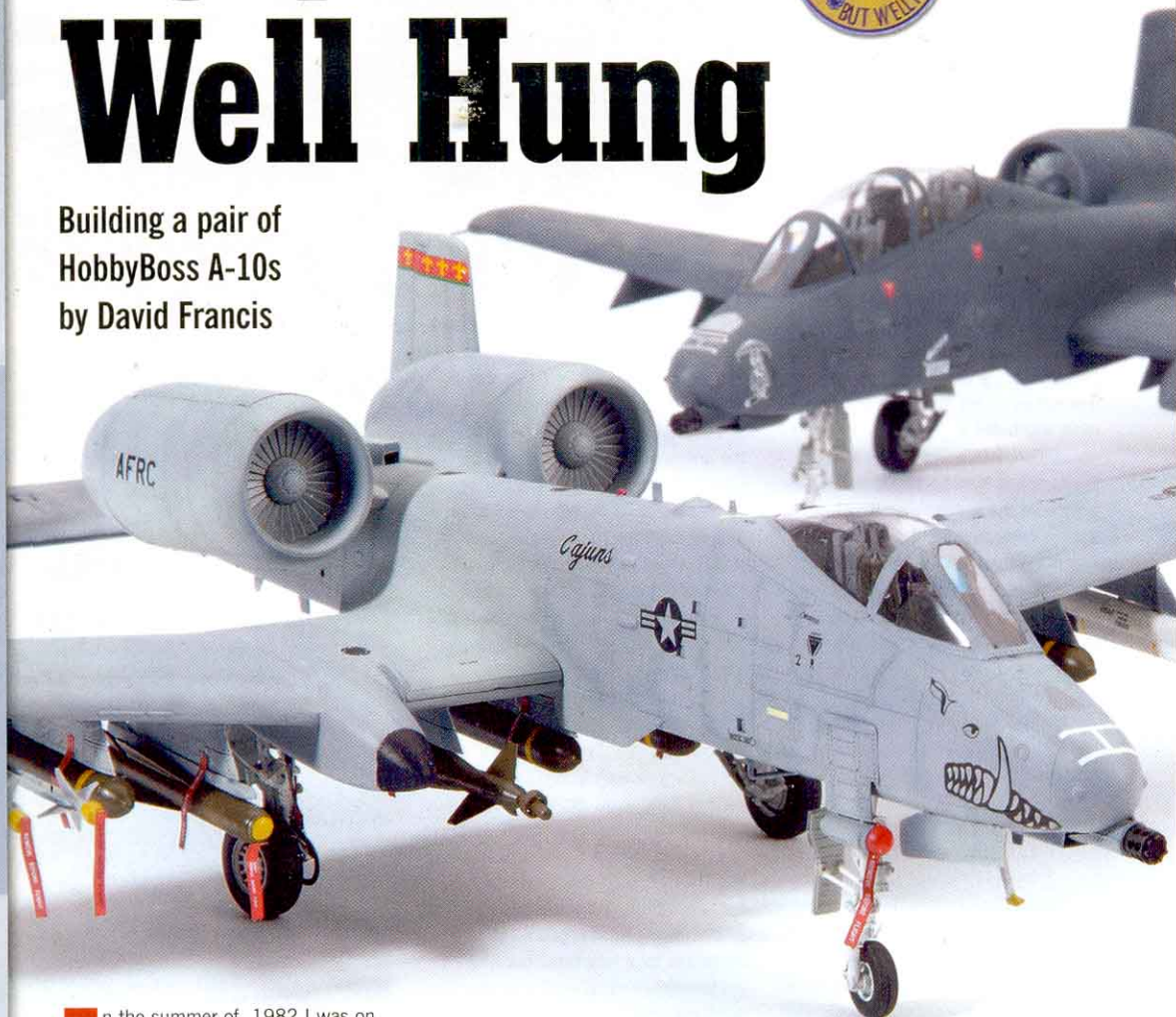
Scale: 1/48 Kit No: 80323
Price: £25.99 Decal Options: 2
Panel Lines: Recessed Status: New Tooling
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Ugly but Well Hung



Building a pair of
HobbyBoss A-10s
by David Francis



In the summer of 1982 I was on my first holiday away from my parents with my girlfriend, Come on Eileen was the hit of the summer and I fell in love on a sand dune on the East Coast and strangely my girlfriend was not with me at the time?

Let me explain, I had gone off on my own leaving her sunbathing next to the swimming pool to visit a near-by military range, as I had always lived in North London and I had never actually seen a real military aircraft outside of a museum and I was hoping that there might be some activity on the range as in those days there were a large number of military bases all over East

Anglia. After an hour when nothing stirred except the occasional sea bird I heard a muted whine and low down on the horizon appeared two aircraft, far from the sleek shape I was expecting seeming to be all wings and engines, a load ripping sound and the front of the aircraft disappeared in smoke as it fired its cannon at a target on the range.

This was my introduction to the A-10 Thunderbolt II, though to everyone associated with the aircraft it's the Warthog or just Hawg.

I have waited nearly 25 years to see a state of the art kit and when Italeri announced an all new tooling I thought that at last my wait was over, however on opening the Italeri box I was disappointed as there were some basic

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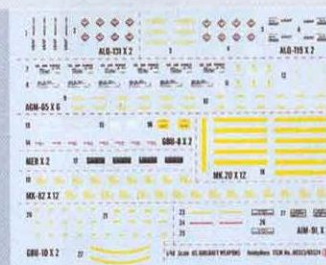
Firstly many people have said that these kits are Trumpeter's 1/32 A-10 scaled down. This is not the case, though there are many similarities.

The box is absolutely packed with plastic and just like a Trumpeter kit the clear parts, vinyl tires and ejector seat are packaged in their own small box, and also just like the Trumpeter A-10 there are loads of weapons more than this aircraft could carry in one go and still take off. These include a fully detailed multi-part Avenger cannon which would look good displayed next





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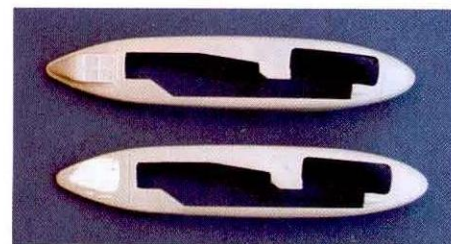
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Modifications to the wheel wells for the A-10B



Comparison of the single-seat and taller two-seat fins



It's simple to modify the rear fuselage to open the APU vents



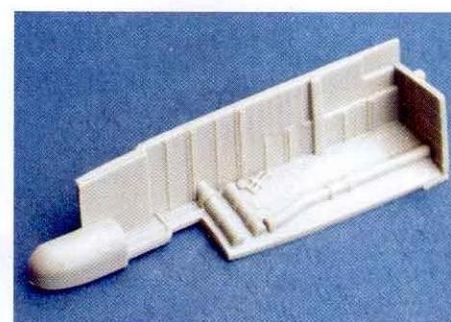
And to the wing tips



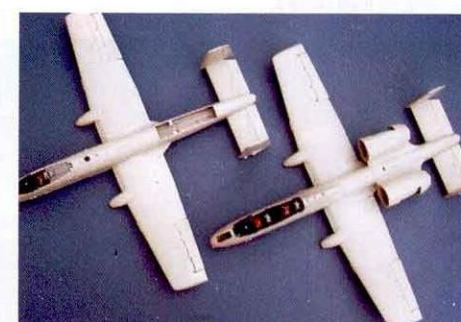
Tissue and PVA mixed with water make the canvas cover



Completed single- and two-seat tubs



Moulded detail in the nosewheel bay



The two-seater's longer nose is clearly seen here

to your Hog as there is no way of seeing it inside the model without resorting to some plastic surgery.

The Trumpeter kit had a few inaccuracies which had to be corrected and none of these errors are present on the new Hobby boss kit; the only initial criticism I had was the nicely recessed engraved detail, which does not really represent the heavily riveted effect seen on a Hog's rear fuselage.

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Construction

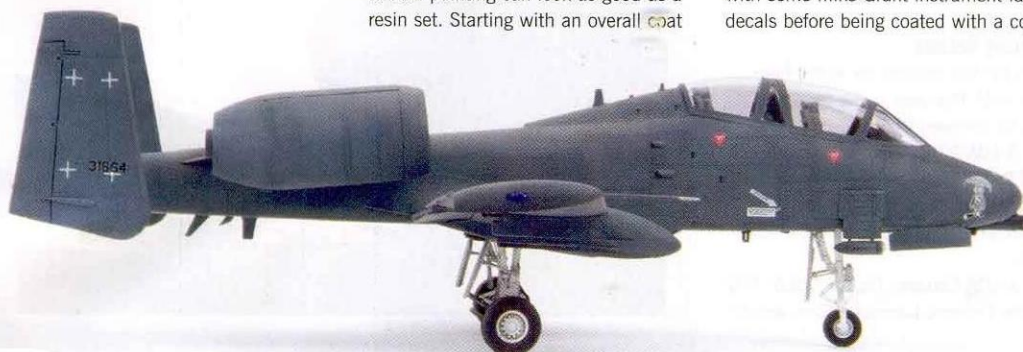
I started by carrying out a few small modifications to the rear fuselage: one of the errors on the large scale A-10 was the positioning of the APU intake and exhaust. On the Hobby boss kit they are in the right position, but the intake is represented as an engraved circle and the exhaust itself is too shallow and missing the baffle fitted above it from the mid-80s onwards. It is a work of a few minutes to use a small drill to open up the holes before adding some plastic tubing to the rear faces to give an impression of depth, and at the same time I produced a new baffle from a small piece of thin plasticard.

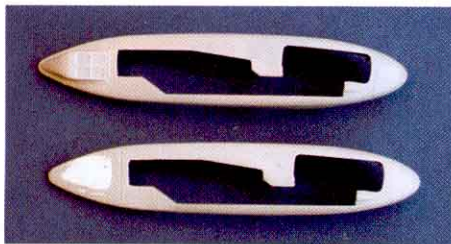
The cockpit is very nice and with careful painting can look as good as a resin set. Starting with an overall coat

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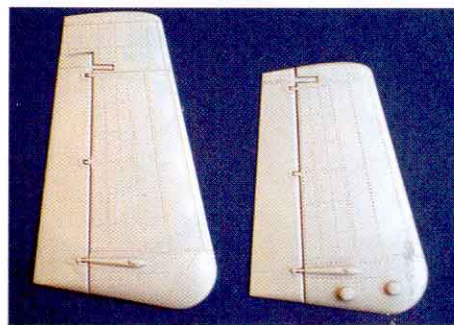
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My attention now turned to that 30mm Avenger Cannon. The kit supplies the large ammunition drum, the breech linkage and the long rotating barrel, the real thing is the same size as a VW Beetle but would be completely hidden in the fuselage, but fortunately Hobby boss have thought of this and have supplied two barrel ends so you can fix one to the aircraft and the other to the end of the cannon displayed beside the plane. These ends feature some engraved circles, but if

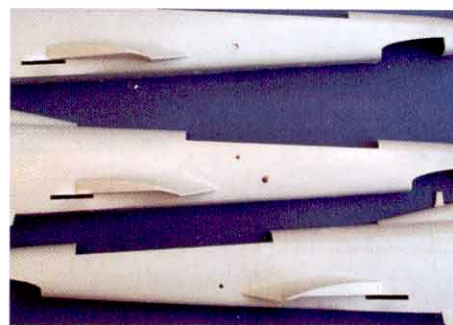




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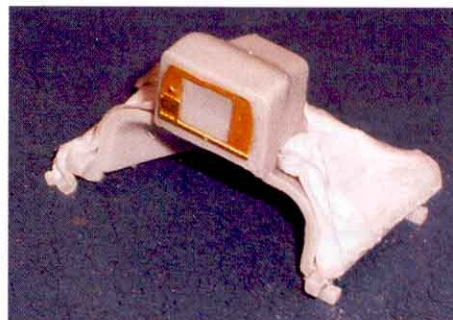
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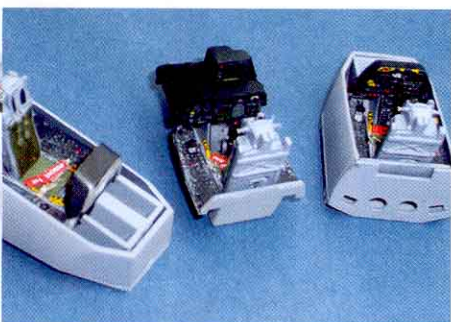
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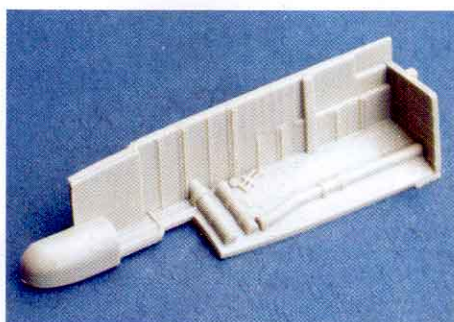
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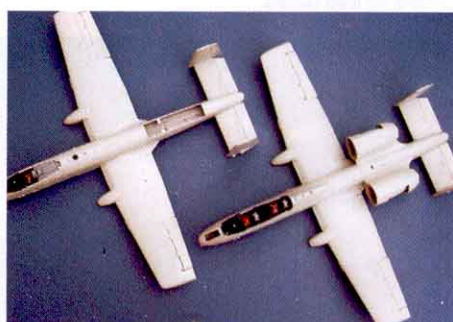
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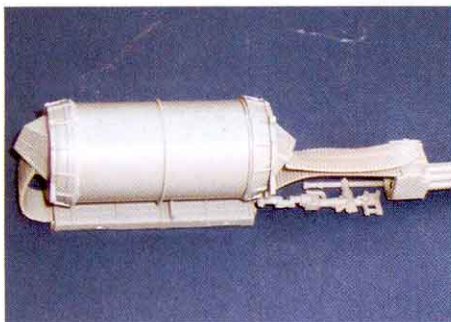
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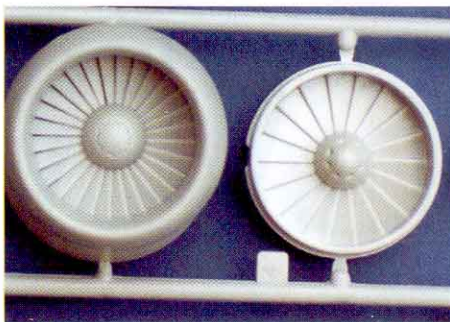
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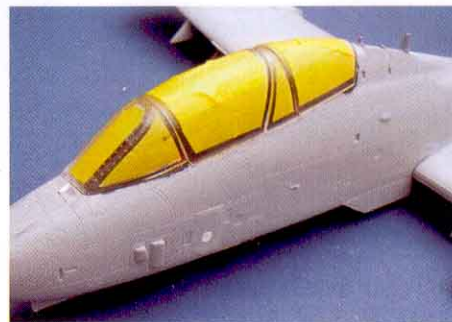




As big as a VW Beetle, the 30mm Cannon



Comparison of the Italeri and HobbyBoss fan blades



Tamiya tape masks are easy to make



Wheelwells are filled with damp kitchen towel before applying the main colours



Paint it black



Pre-shading in action



Looking good ready for decals



Desert-faded two-seater



you are careful you can use a small drill to open up the circles which looks far better on the model.

It was now time to join the fuselages not forgetting to add a considerable amount of nose weight, a useful tip here is to leave off the panel on the top of the nose until later in assembly. Now if you find you have not added enough weight you can add a little more into the end of the nose.

From this point assembly progressed remarkably quickly with only tiny amounts of typewriter correction fluid being used as filler, normally around the area where the parts were attached to there sprues. Only two areas need a little work, the first are the separate wing control surfaces. I found that you may need to trim some of the parts to get a snug fit, and it is a similar story on the engine nacelles. These are designed to be posed either open or closed but as little engine detail is supplied I will wait until the aftermarket address this area before building an A-10 with them open.

The fit is a bit variable but with some

test fitting and a little trimming you can get a good fit. One area that I must mention is the large fan at the front of the intake, which is a major weakness on the Italeri kit having too few blades. HobbyBoss have this area spot on. What is more amazing is that if you hold this part up to the light you will find that the fan blades are almost separate parts and with careful painting look stunning.

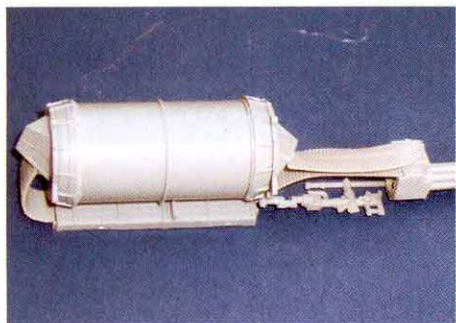
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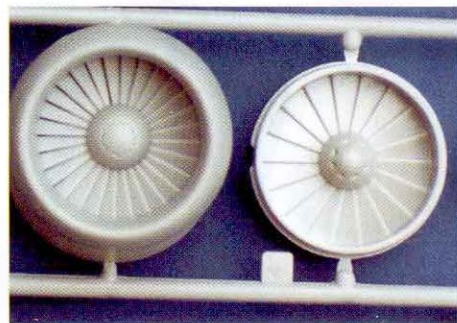
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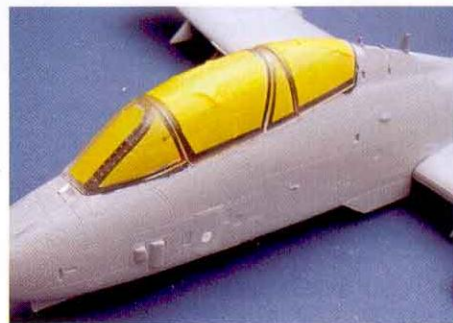




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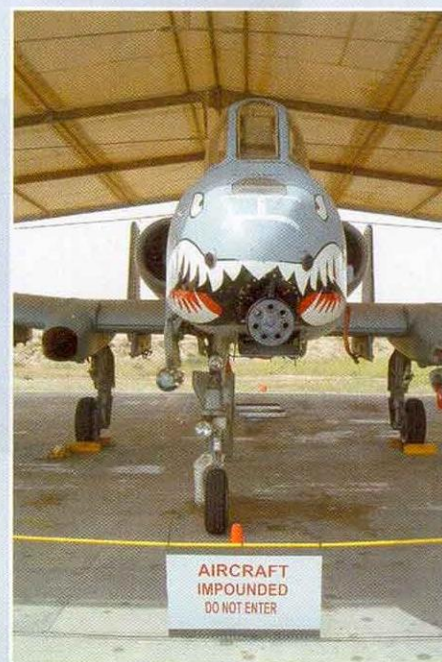
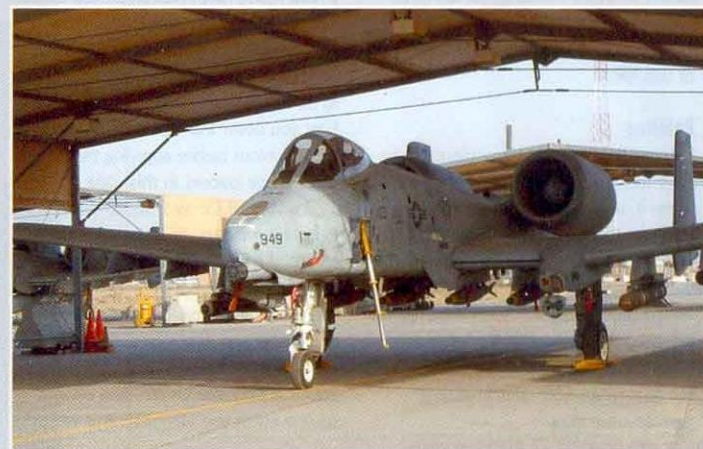
The Hogs of War

The tank-busting A-10 Warthog returned to haunt the Iraqi Army with another display of its killing prowess during Operation Iraqi Freedom. Ahmed Al-Jaber Air Base became 'Hog Heaven' as the US Air Forces A-10 Thunderbolt IIs took up residence for the duration of the War. Using the unerring accuracy of their Maverick missiles, and the power of their General Electric GAU-12 gun, spewing out milk bottle sized depleted uranium shells, resistance was futile!

The A-10 also proved its abilities to take punishment and still get its pilot home as Captain Kim 'Killer Chick' Campbell of the 323rd Expeditionary Wing discovered when her Hog was badly shot up by Iraqi AAA fire!

Many A-10's sported mission and kill markings along with 'door art' concealed inside the ladder access panel, and once again the 'Warthog' proved that in its dedicated field it has no equal. As the Hog Drivers say 'Go Ugly.'

Pictures via Andy Evans



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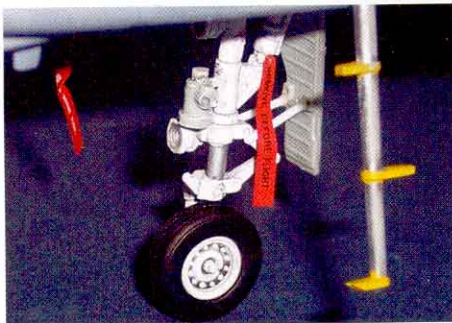
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RBF Tags and open ladder bay add interest



RBF Tag in the cockpit draws the eye



A little added humour to the bomb load

used its 30mm cannon to destroy an Iraqi MBB 105 Helicopter during *Operation Desert Storm*.

Since its entry into service the A-10 has received a number of upgrades and the kit has many of these upgrades featured including the LAST (Low-Altitude Safety and Targeting) and GPS antennas, which were only applied to aircraft in the late 90s. This is not a major problem as it is fairly easy to remove them with a sharp knife and some filler if you wish to model an early aircraft.

But in my case I wanted to have a model in the current grey schemes so I purchased Twobobs sheet TB 48-054 which provides markings for an A-10 operating from Bagram during *Operation Enduring Freedom*, but more of this later.

Painting

I started by painting the undercarriage bays using Humbrol Gloss light grey. As there is some nice moulded detail in the bays I applied a light wash of heavily thinned gunblack oil paint to lift out the detail, and at the same time I added the prominent pipe visible in the left gear bay that leads from the single-point refuelling point through the bay to the rear wing, from plastic tubing painted dark grey.

I now masked all the clear parts with Tamiya tape before spraying the



Almost there but needs some ordnance

whole model with Halfords matt black primer, why I hear you ask?

I have recently become a convert to pre-shading my models and an easy way to do this is used by armour modellers. First you cover the model completely in a dark colour before applying the camouflage colours in this case Humbrol 127 and 128 thinned 50/50 with Humbrol thinners. You start by filling the centre of each panel with your main colour; once you have completed this your model will look like a patchwork quilt. Now you lightly mist on more coats of the original colour until the dark colour is only just visible, and once you have a finish you like, stop and leave

your model to dry overnight. If you think next day that you need another misting coat you can apply it but normally I find that if I stop as the undercoat is just disappearing completely I have it just right. Now the final stage is to slightly lighten your original top surface colour and apply this to the centre of all panels that are faded by sunlight, like the tops of the wings, fuselage and engine nacelles. Again trust your instincts, stop as soon as you think it looks OK, an over-weathered model is just as bad as an immaculate one.

The Humbrol colours I used dried to a nice stain finish which enabled me to move straight on to the decaling stage before finishing with a final coat of Xtracrylix matt varnish.



The finished A-10B

The Decals

The kit decals are of the normal quality that we expect from a Far East manufacturer being in perfect register but a bit thick compared to aftermarket standards, though they do work well if you use Gunze Sangyo decal solutions available from MDC, as these seem to be a bit stronger than those sold by Microscale.

I used the kit decals on the weapons and the two-seater featured later with no problems, and the Twobobs sheet also achieved equally successful results.





The Final Straight and those weapons

The undercarriage shows a good level of detail and includes all the retraction jacks that link the doors to the legs. I used the wash technique to lift out the moulded detail and I also applied a selection of data placards using stencils cut away from leftover decal sheets.

Two areas of this model that I particularly like are the separate pilot ladder and the open refuelling knuckle on the front of the port wheel well; details like this add that little bit of life to your model shelf and also increase the possibilities for any modeller wishing to produce an airfield diorama.

I have already mentioned the large quantity of ordnance provided in this kit, and there is enough supplied to arm at least two 1/48 scale models so there will be some nice additions to your spares box. You can mix and match from the following selection:

- 2 X AIM-9L
- 6 X AGM-65
- 2 X GBU-10
- 2 X ALQ-131
- 12 X CBU Mk20
- 12 X Mk82
- 2 X GBU-8
- 2 X ALQ 119

and finally two fuel tanks. The Hellfire missiles have separate clear seeker heads and you can either paint these to represent the cover fitted until just before flight or use a small disc punched from holographic wedding confetti to represent the seeker head, I also used this material on the cockpit HUD and in the Pave Penny as I like the look it adds to my model.

From this large array of weapons I chose a selection based on photographs of A-10s in operational service over Afghanistan to equip my model.

With the model almost complete I decided to go to town with the RBF tags and again using photographs from the web adorned both the weapon, pylons and airframe with Eduard tags.



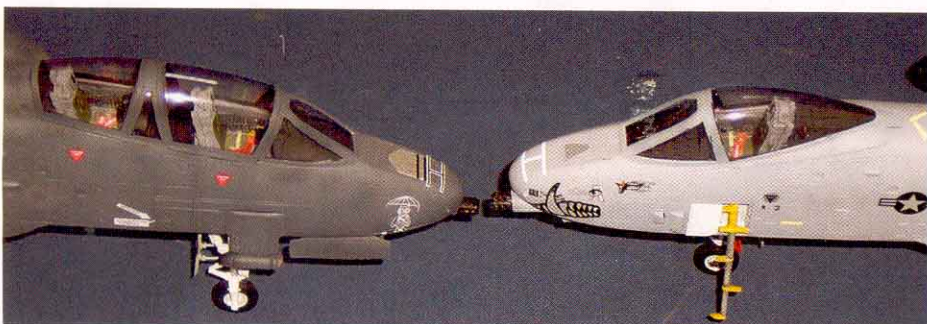
More RBF Tags



Thirty years separates these two aircraft



If only all my models looked this good?



One seat or two?



Drilling out the gun barrel is worth the effort

On real aircraft you will see large variation in style of the tags used and these can be easily copied by mixing the different lengths and colour prints that Eduard supply on each set. One other aftermarket item used was set of Airwaves seeker head covers which are inexpensive at only £2.95 for six, which also includes a selection of paper RBF Tags.

And that was the single-seater, so now for the two-seater.

Two-up Hog

The only two-seat Hog was produced by Fairchild from the single-seat prototype but unfortunately failed to find a buyer. Personally I think this is the more attractive of the two aircraft but unless you enter the world of what ifs, you are limited to the overall grey scheme that the A-10B carried for its whole life, though the desert conditions at Edwards AFB did lead to some large areas of fading and weathering.

The construction is very similar to the single-seater, with the following additions:

- 1 There is a small moulding seam on the lower fuselage that needs to be removed with a sharp scalpel.
- 2 On the rear lower fuselage the instructions have reversed the position of a large aerial and the fuel dump pipe.
- 3 You will need to remove all of the flare pods moulded under the wings and behind the wheel wells. This is delicate operation so go slowly so you can avoid damaging the surrounding area, I would suggest filling the rear

two-seater is poorly represented; this was made of a canvas cover surrounding a single VDU screen. I added an etched brass screen from the spares box to the plain plastic square cover in the kit. An easy way to reproduce the canvas cover is to cut a piece of tissue paper to an approximate shape, then dip the tissue into a solution of PVA wood glue and water while still damp add this to your model insuring that you add some realistic looking



of the flare pods with Milliput a couple of days before you start work as this will strengthen the plastic as well as supply a base to which any future filling can attach.

- 4 Unfortunately the Laser and Radar pods carried by this aircraft are not supplied so will have to be scratch built, though there are numerous photographs of the aircraft flying without them. Despite a lot of searching I have not found any photographs of this aircraft carrying weapons.
- 5 The top of the rear section of the



creases. After leaving the cover to dry overnight you will find the tissue has gone hard and you can trim off any excess with a sharp knife before painting.

- 6 The Owl marking on the nose was only carried in later life when the aircraft was starting to show quite a lot of weathering.
- 7 In some photographs a long test probe is protruding from the refuelling probe on the upper nose, this is not supplied.

Conclusion

These are two of my favourite models of all time and I have already added another three kits to my pending pile. The mouldings are to a very high standard and the potential is there for a skilled modeller to make a real jaw-dropper. On the downside all those extra weapons and fully detailed cannon have led to a fairly high retail price of £25.99 in the UK, which is an issue as both the Italeri and much older Monogram Hawks are available for at least £10 cheaper.

In my view this is the best A-10 on the market and if this is the kind of quality we can expect from HobbyBoss I look forward to seeing future release from this range, especially the recently announced family of Grumman Wildcats.

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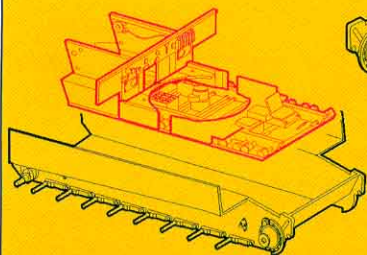
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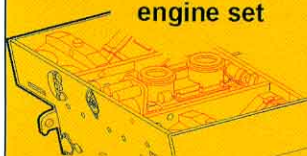


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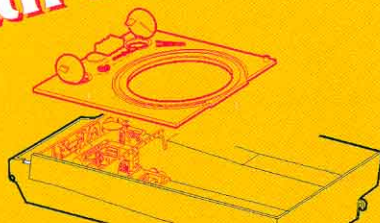


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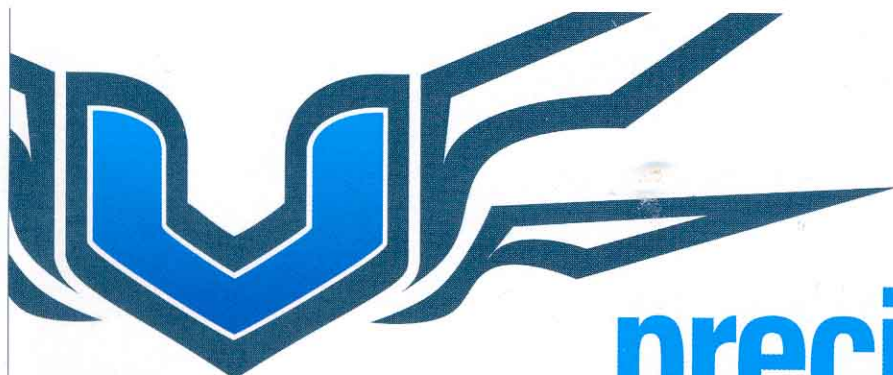
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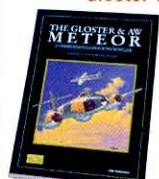
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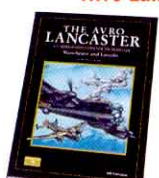


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Scaling Down Dragon

The F/A-18E Super Hornet in 1/144

The proof of the pudding, so they say, is in the tasting. Following Garry Prettyman's superb 1/48 Diamondback in last month's SAMI, it seemed only right and proper that Scaling Down should put its money where its mouth is and attempt to bring the type down to size. Once again, Gary Hatcher gets out the filler and the grey paint...

When Dragon's 1+1 series started turning up in the review samples there were mixed feelings around the editorial table. 'Some interesting subjects,' it was mooted, 'but a lot of the kits are dated.' Thus we approached the first arrival with caution. It was a double boxing of 'Jolly Rogers,' featuring an F-14B Tomcat and an F/A-18F Super Hornet. Eyebrows were raised: 'a Super Hornet simply can't be an old tooling' we pondered.

It wasn't, and isn't. Discarding the Tomcat, which is the original 1980s kit with new weapons and an, admittedly, superb decal sheet, we seized upon the Super Hornet sprues and whisked them away for appraisal.

This kit, #4591, was followed up shortly by two more 1+1 boxings. These were #4590 – which was presumably the first actual release – and #4597. The former featured two F/A-18Es in VFA-143 markings, while the latter was a very tasty package containing two F/A-18Fs in



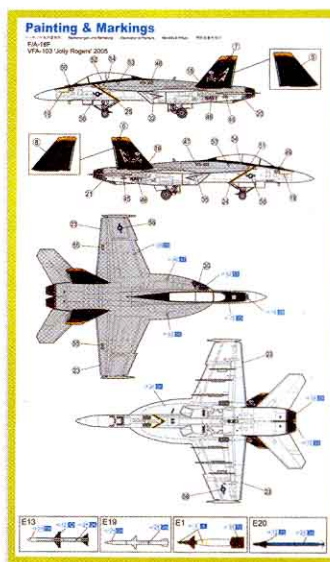
Boxart for the kit presented here

Black Lions markings, complete with additional buddy tanking pods.

These kits look really nice. Engraving is similar to a Revell kit, weapons are plentiful and very nicely moulded, with the neatest scale Sidewinders yet seen in 1/144, and the decal sheets feature one lo-viz and one hi-viz option, so all tastes are catered for.

Filling in Time

Construction is, initially, swift and simple as there are not so many pieces. Dragon have cleverly tooled the intake parts to allow for some nice sharp lips, and the only slight problem here is that the instructions are not entirely clear as to



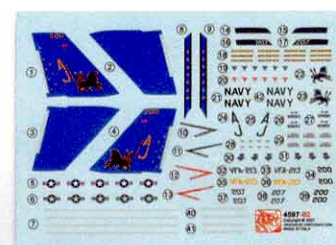
Instruction sheet for the Jolly Rogers' F/A-18F

the correct position of parts A43 and A44. As long as you remember the thin edge needs to be at the mouth of the intake, then the ambiguity can be avoided. Parts are tooled to allow for both single- and twin-seat versions, and this does not cause any difficulty. Problems that do crop up when you come to assemble the forward fuselage are a result of the appalling fit of this sub-assembly – in fact, fit in general is poor, and a lot of filler will be needed, and a fair amount of engraved detail lost. Fortunately this is all on the underside, so does not constitute too great a compromise to the finished model. It is advisable to make the best fit possible on the top of the fuselage, in front of the canopy, and leave as much of the clean-up work underneath as possible.

Perhaps the biggest fault with these kits lies in the manufacturer's decision to provide a folded wing option. This may seem attractive at first glance, but the hugely overscale hinges and poor fit make it virtually impossible to model



Boxart for the attractive Black Lions pair – kit no. 4597



Decal sheet for #4597. No FS number is indicated for the blue upper fuselage. Answers on a postcard please



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Filling in Time

Construction is, initially, swift and simple as there are not so many pieces. Dragon have cleverly tooled the intake parts to allow for some nice sharp lips, and the only slight problem here is that the instructions are not entirely clear as to



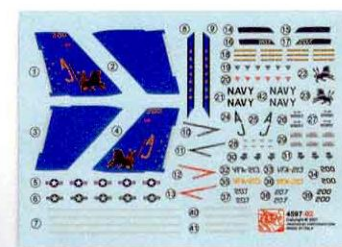
Instruction sheet for the Jolly Rogers' F/A-18F

the correct position of parts A43 and A44. As long as you remember the thin edge needs to be at the mouth of the intake, then the ambiguity can be avoided. Parts are tooled to allow for both single- and twin-seat versions, and this does not cause any difficulty. Problems that do crop up when you come to assemble the forward fuselage are a result of the appalling fit of this sub-assembly – in fact, fit in general is poor, and a lot of filler will be needed, and a fair amount of engraved detail lost. Fortunately this is all on the underside, so does not constitute too great a compromise to the finished model. It is advisable to make the best fit possible on the top of the fuselage, in front of the canopy, and leave as much of the clean-up work underneath as possible.

Perhaps the biggest fault with these kits lies in the manufacturer's decision to provide a folded wing option. This may seem attractive at first glance, but the hugely overscale hinges and poor fit make it virtually impossible to model



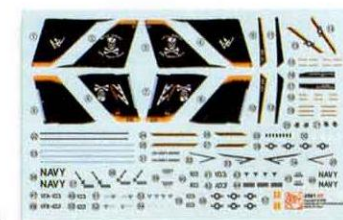
Boxart for the attractive Black Lions pair – kit no. 4597



Decal sheet for #4597. No FS number is indicated for the blue upper fuselage. Answers on a postcard please



Boxart for Jolly Rogers. Buy it for the decal sheet alone!



Jolly Rogers decal sheet. Hopefully the F-14 markings will transfer to one of the Revell kits waiting in the wings!

them in the open position without a great deal of sanding and filling. This seems to be a problem with a great many kits in a variety of scales. Wing fold options are all very well if the manufacturer is providing adequate parts to depict the mechanism accurately, but in this present instance there is absolutely no detail whatsoever, and the gaps left when posing them open require major attention.

Part of the problem is caused by the many panel lines, very close together, that cross the wing fold. These need to

be rescribed after the gaps have been filled and sanded, and the resulting labours required more time and attention than the rest of the build altogether.

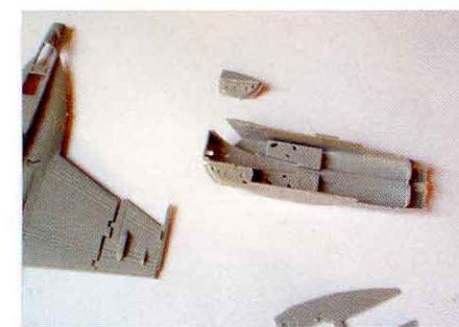
The canopy was a reasonable fit, but the join is too noticeable in this scale. I ended up filling it all the way round and sanding it smooth, something that you can get away with in 1/144, but not with anything larger.

Attention to Details

Gimmicky wing fold and poor fit aside, there are many excellent aspects to

these kits, and in all fairness the sanding and filling is no more than I expect from any model I build. Show me one of these 'shake and bake' kits I read about and I will gladly build it. Maybe it's me, but I reach for the filler every time.

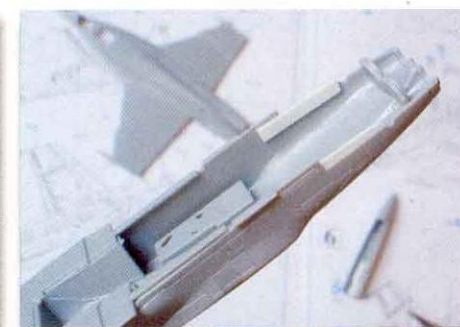
The stores provided are excellent. Solid one-piece wing tanks require a minimum of clean up, and all the missiles are finely detailed. No decals are provided for stores, so I borrowed some stripes from a Revell Tomcat to liven things up, and filled all nine stations on the finished model. A little



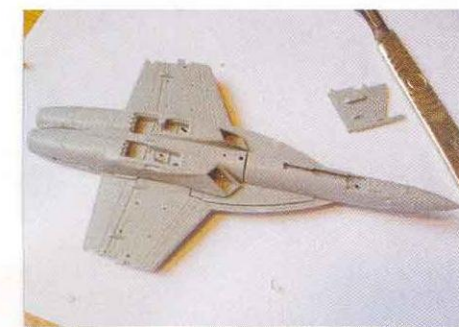
The fit of the top parts of the intakes – A43 and A44 – is unclear in the instructions. Make sure the side with the holes on goes on top



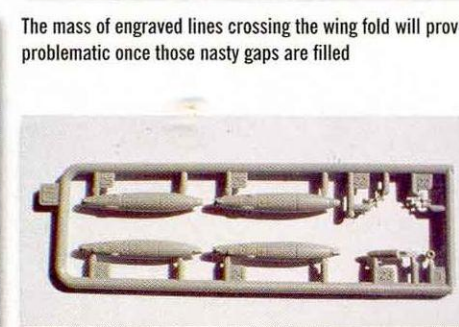
The mass of engraved lines crossing the wing fold will prove problematic once those nasty gaps are filled



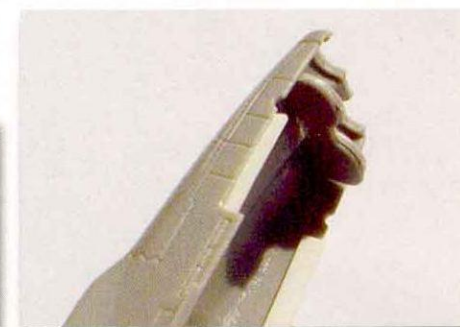
Some plastic strip was used to reinforce the joint at the rear end of the split fuselage



Poor fit all round with the main airframe components



Refuelling pods from the Black Lions boxing. Parts are provided to build four complete items

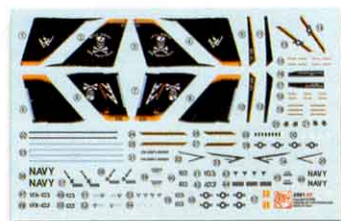


Don't forget to fit part A31 into the rear fuselage, and a lot of weight into the nose!





Boxart for Jolly Rogers. Buy it for the decal sheet alone!



Jolly Rogers decal sheet. Hopefully the F-14 markings will transfer to one of the Revell kits waiting in the wings!

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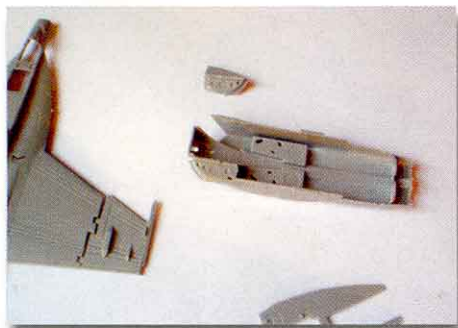
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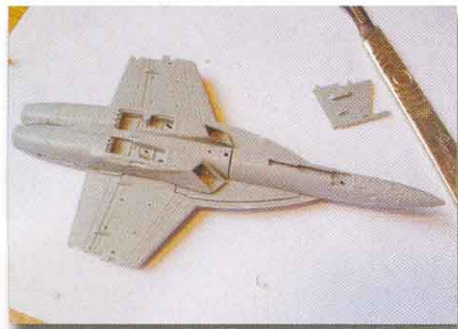
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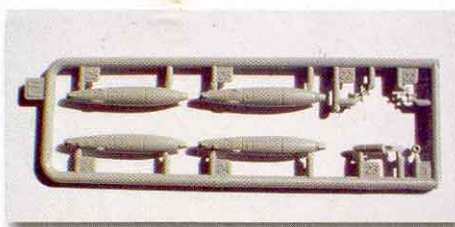


Some plastic strip was used to reinforce the join at the rear end of the split fuselage

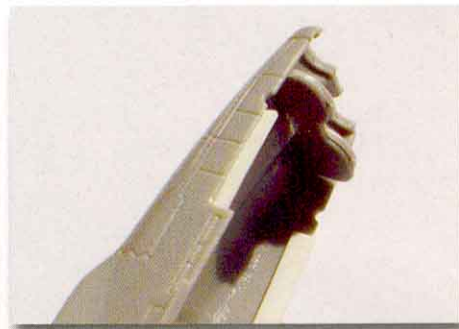


Poor fit all round with the main airframe components

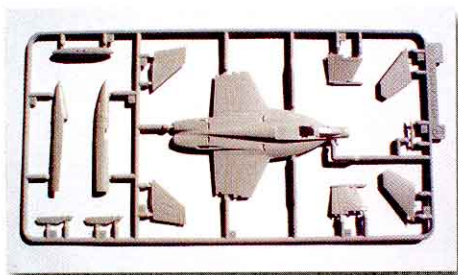
The mass of engraved lines crossing the wing fold will prove problematic once those nasty gaps are filled



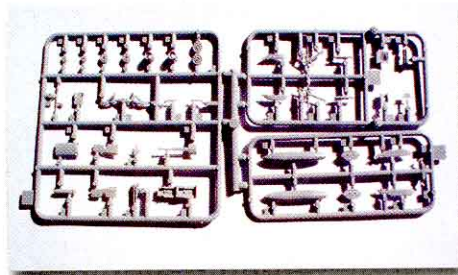
Refuelling pods from the Black Lions boxing. Parts are provided to build four complete items



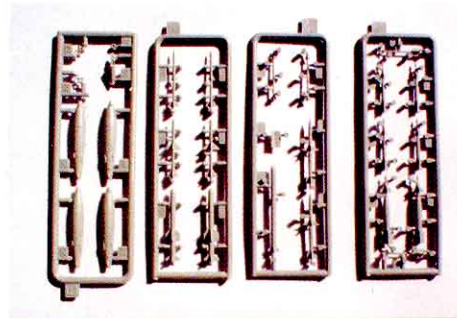
Don't forget to fit part A31 into the rear fuselage, and a lot of weight into the nose!



Sprue A from kit 4590



Sprue B. All sprues are duplicated in the 1+1 boxings, which gives you a spare nose wheel leg with each kit, as well as plenty of stores for the spares box



Stores and weapons from kit 4597



time spent on these will pay great dividends, and on the next F/A-18 I build I will use the buddy refuelling tanks provided with the Black Lions kit.

The undercarriage is another nice piece of moulding, and all went together with little problem. The front wheel well is too narrow towards the rear, and a little trimming was required on the leg, but it all looks the part.

Shades of Grey

Being partial to the current lo-viz Navy schemes, I opted for the beautifully subtle tones of Lifecolor UA026 and UA027, for the two very similar shades of grey. I sprayed the model all over with the paler shade – 026 – then used an Iwata Eclipse to apply the top colour using a pre-shading technique. I sprayed fine lines around the edges of the wings

and along the panel lines, without filling in at all, and the close proximity of the two colours left me with a subtle weathered effect – possibly too subtle for the camera to reveal, but it looks nice in the flesh, honestly!

Decals went on with no problem whatsoever. I am a little uncertain about the yellow strip lights, but the overall effect is what I was aiming for. There was no silvering, and all the images were printed nicely and in register.

Conclusions

So, what is the verdict? Is it possible to 'scale down' the F/A-18 and get a

satisfactory result?

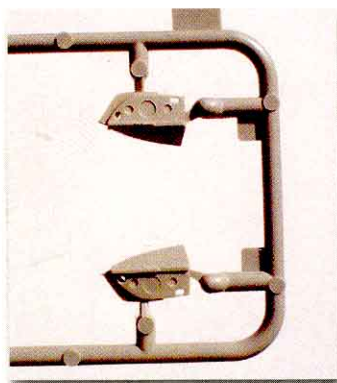
The answer is a resoundingly non-committal *'that all depends.'* If you are looking for the kind of detail achievable in 1/48, then that was never my intention. If you want to present a model that looks good, will form part of an attractive collection, and still looks better than anything yet made in diecast, then I'd have to say that with Dragon's kit I built a model certainly no worse looking than I would have in 1/72. That may sound like a cop out, but the reader will have to look at the result and make up their own mind.

Personally I am happy with it. The

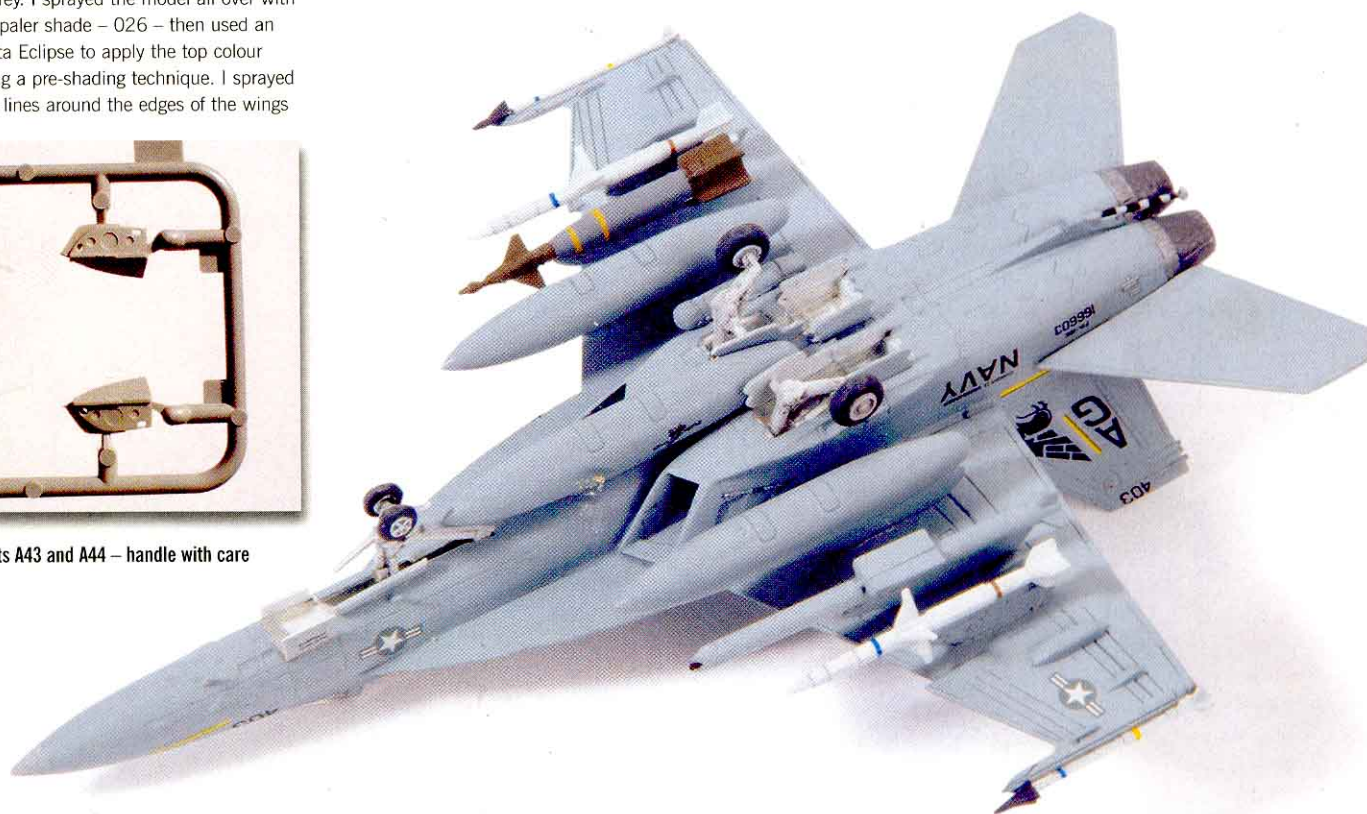
only bit that nags is the area across the wing fold line, where I had to mess about so much with filler and sanding. This aside it looks a beauty to me, and I eagerly anticipate adding the two-seater before tackling the pair of A-6s that turned up the other day in lo-viz *Desert Storm* markings.

If the proof of the pudding really is in the tasting, Dragon's 1/144 Super Hornets are not quite Banoffi Pie – that wing fold really is a nuisance – but certainly a very superior brand of profiterole!

SAMI



Parts A43 and A44 – handle with care



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Boeing V-22 Osprey

Scale: 1/48 Kit No: 2622

Price: £17.99

Type: Injection Moulded Plastic

Manufacturer: Italeri

UK Importer: The Hobby Company

Osprey or Albatross?

The Vicissitudes of the Boeing V-22, described (and endured) by Tony Grand

Sometimes you think you'll never build a model of an aeroplane that really interests you. Kit in wrong scale. Too expensive. Too many other projects in hand (read: unbuilt kits piling up). Not enough reference material (read: only half a tree's worth and several dozen websites bookmarked). But then comes the spark and you rush out for the kit and start building.

So it was with me on this occasion. I subscribe to only two aircraft magazines, *SAMI* and *Aeroplane*, but that doesn't stop me occasionally looking at other publications. In this case it was *AirForces Monthly*, which had a rather nice piece about the US Marine Corps working up the Boeing MV-22 Osprey prior to combat deployment, which will be with VMFT-263 to Iraq in September.

I'll come clean: I really like the

Osprey. To slip into the vernacular, I think it's a cool bit of kit. I mean, look at the spec: max airspeed 0 to 340 knots; cruise 250-300 knots; max ceiling 25,000 feet; internal cargo 20,000 pounds; external load on dual hooks 15,000 pounds. If you compare that to the specification of the 50-year-

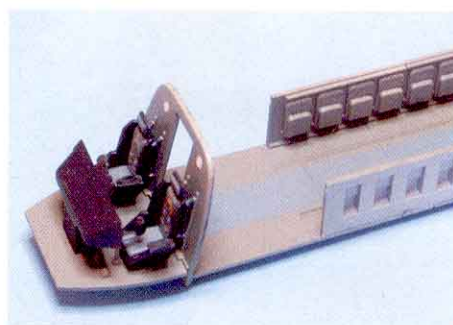
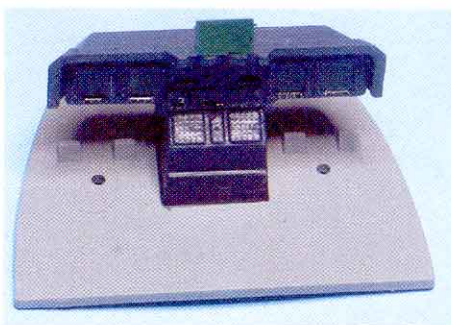
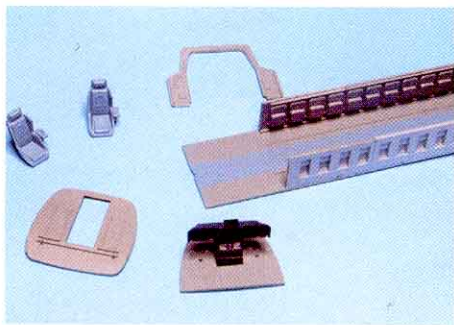
old H-46 Sea Knight as set out in the *Big Boys' All the World Planes* you'll see why the USMC has fought tooth and nail to make sure it gets the V-22 as a replacement.

But if you Google the Osprey, the majority of the URLs are about the controversy surrounding this amazing

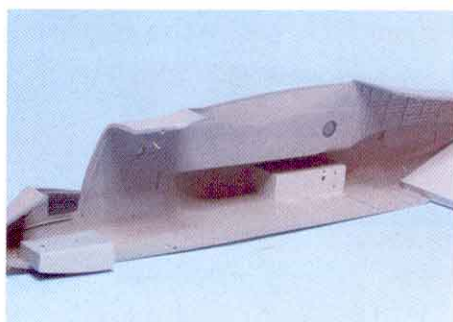
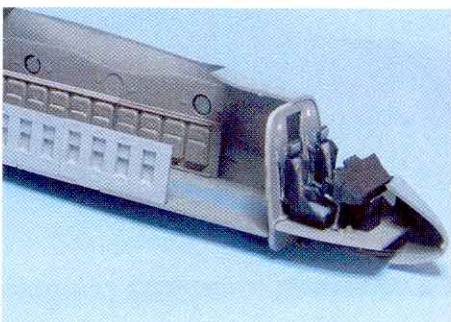
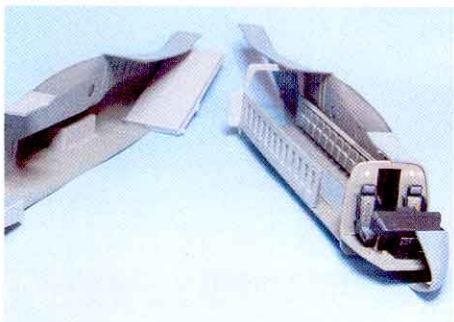


References

- Bell Boeing V-22 Osprey, by Bill Norton, Aerofax 2004
- Prop-Rotor Ops: *AirForces Monthly*, March 2007
- Osprey Lifts Off, *Flight International*, 25-31 July 2006
- United States Marine Corps Airpower Update 2007: Combat Aircraft, March 2007
- The Helicopter Page. www.helicopterpage.com



"The build starts with the interior though you could get away with leaving it out as Italeri, in their wisdom, have tinted all the transparencies to pop-star anonymity level"



Work progresses on the cabin area, which Tony chose to model closed-up

bird. The, to me, most entertaining reference was a 'paper' (a term I personally reserve for something with a little more rigour) by a retired USAF Colonel, which really should have been written in green ink and capitals. I put his rather slighting tone down to envy that the Marines are getting the odd 360 of these planes, whereas the USAF is only getting 50, for SPECOPS (special operations). Oh, and one link was called 'The V-22: Osprey or Albatross?' (whence my title). I imagine you can guess the authors are agin it. They believe the V-22's advantages over existing helicopters are marginal. Tell that to the Marines?

Many of the Web references are political in nature and I suspect it's so-called 'pork-barrel' politics, which, apart from the USMC's commitment and the pragmatic point that starting a new programme was not an option, has saved the Osprey. (The work on it is spread over some 43 States.) It's fair to say this aeroplane has been round the block a few times. Development contract awarded 1985; first flew 1989; a fatal crash in 1992; cancelled then reinstated 1992; programme stood down in 2000 during the first OPEVAL (operational evaluation) after two fatal crashes. In 2005, the Osprey was finally approved for full production, after a return to flight programme in 2003 and OPEVAL in 2005.

The Build

You're not exactly spoilt for choice with Osprey kits: one in 1/72 and one in 1/48 scale, both by Italeri. I'm not sure if the 1/72 is current, as Hannants don't list it. The 1/48 kit I decided to model is dated 2003 on the box. It gives you one option only: MV-22B aircraft 13, which took part in the first

OPEVAL. The kit instructions give 2001 as the year in which the aeroplane is represented; technically true, I guess, though the fleet was stood down in 2000, as we have seen.

The Osprey as it's flying now does not differ externally from the 2000 version, so you could build a current 'bird' if you could locate decals. I couldn't. I haven't got any scale plans of the V-22, but the length of the kit scales out exactly, so that, and the fact that most detail I've compared with the extensive photo coverage in Bill Norton's Aerofax volume looks good,

gives me confidence. (With Norton's book and the Flight International cutaway I cite in the references you could build your own 1/1 scale V-22....)

Interior

The build starts with the interior; a through cockpit and cabin structure, though, to be honest you could get away with leaving it out, as Italeri, in their wisdom, have tinted all the transparencies to pop-star anonymity level. A feature not evident on the original, it must be said, as you can see right through. However, with Bill

Norton's pictures, and those by Paul Bird on the IPMS London website, you could reproduce both the cabin and the 'troop-carrying' area to a very satisfactory standard, if you want to leave the ramp down, as there's a good basis. (The real masochists amongst us could, I guess, vacform a canopy.) I chose to have my Osprey closed-up, since I wanted this to be a fairly straightforward build, as a foil to a concurrent hair-tearing project. (Amongst other things, the padding in the cabin seemed to me to rather problematic but since building the V-





22, I see in Mike Williams' Hercules build (SAMI, May 2007) how he used Klear-soaked kitchen towel as padding—good wheeze.)

You'll have noted the title of this piece. The vicissitudes don't all lie with the full-scale V-22...

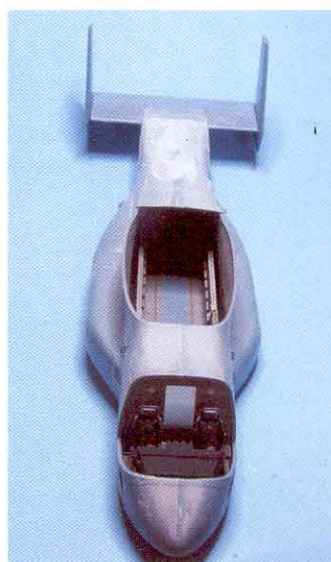
Fuselage, and marks of shame...

I dry-fitted the fuselage several times, thinking there might be problems, as it's a fairly large assembly, and I decided to use superglue. Mistake...Something went wrong as I put the fuselage halves together and I was left with a gap on the underside and a discontinuity, to put it delicately, on the upper side which I couldn't correct, as everything was nice and firm. Out with the Squadron White Putty, which I've only tried recently and found to be very useful for large scale filling on vacforms, as it dries quicker than Green Stuff. However, White Stuff

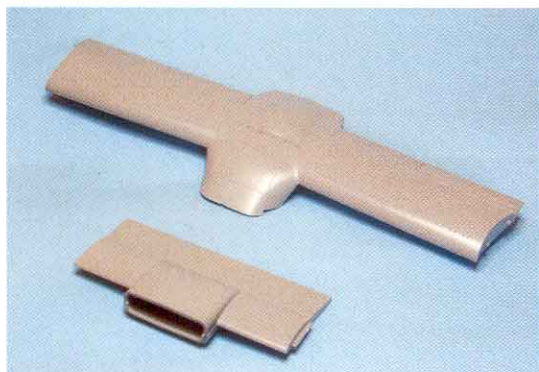
was the wrong stuff here, I think because it doesn't etch into the plastic in the same way as the Green. Also, as I was sanding away, the seams kept cracking open as I applied pressure. I think I've worked out why I had that trouble on this fuselage and not on previous equally large ones. If you press on the join in a round or oval section fuselage, you have the natural strength of the arch. With the join in a box fuselage, that strength is absent and the plastic flexes away. Perhaps plastic tabs or a strip along the joint would have helped, as in a vacform. Anyway, the seam was still visible. I must stress that all this was my fault: the kit is nicely thought out.

Wing and tailplane

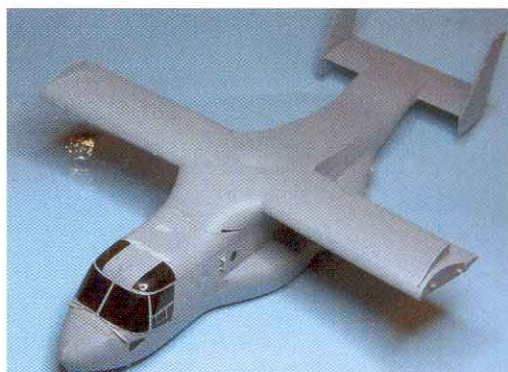
There's not a lot to say here. Wing and tailplane fitted together and went onto the fuselage quite well. I should note that the detail on the kit is of good



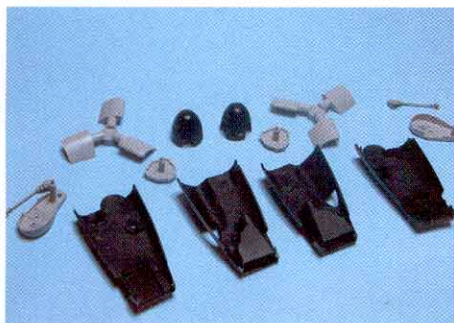
Assembled basic fuselage with 'marks of shame'. "I must stress that all this was my fault: the kit is nicely thought out"



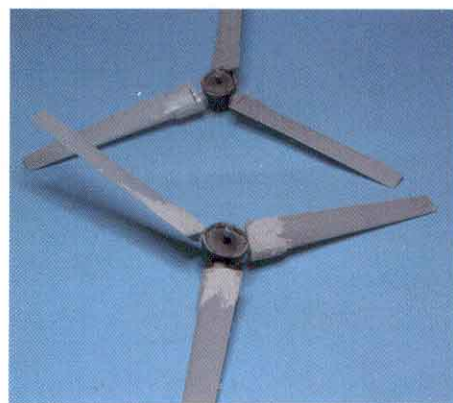
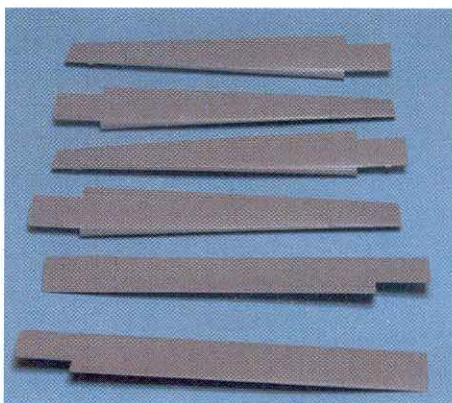
Wing and empennage assembled and installed



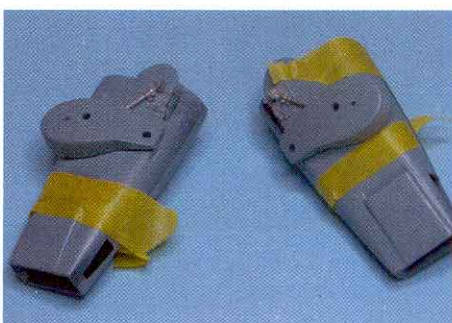
quality. Panel seams are raised but look fine, as do the rivets, which aren't too obtrusive. The small wing fences could do with refining but I couldn't see myself doing that successfully (the same with the airdials round the nose) so I left them. You don't blend the wing into the body, as the whole wing assembly on the MV-22 pivots on what I can only describe as a giant Lazy Susan bearing, to stow fore and aft, to save space aboard ship. According to Bill Norton the whole (automatic) 'blade fold/wing stow' procedure takes 90 seconds in a 45-kt wind on a pitching and rolling deck, or 10 minutes done manually.



"The nacelle/rotor assembly is well thought out, but the fit of the various rotor parts leaves something to be desired"



"There is a gaping hole in the bottom of the nacelles and although you don't see this unless you fit the nacelles in the optional flying position I chose to add a representation of part of the exhaust system"



"There is a nice, if relatively basic, reproduction of the assembly at the wingtips which holds the engine nacelles. You could pretty this up quite nicely"



SAMI giveaway 'brushlet' "works amazingly accurately in small spaces", and is being used here to apply Rub 'n' Buff to a wheel hub

Engine nacelles and rotors

The nacelle/rotor assembly is well thought out, but the fit of the various rotor parts leaves something to be desired. The nacelles went together with no problem although there is a gaping hole in the bottom. Although you don't see this unless you fit the nacelles in the optional flying position (which brings its own problems of display!), I chose to add a representation (scrap plastic and metal mesh) of part of the exhaust system. Bill Norton's book has the necessary photo. There is a nice, if relatively basic, reproduction of the assembly at the wingtips which holds the engine nacelles. Again, you could pretty this up quite nicely. A reference, which I can't lay my hands on at present, states that having the engines there, rather than centrally-mounted engines and drive shafts, is the equivalent of having a Huey on each wingtip! Not far out, dimensionally, at least: Huey main rotor diameter 48 ft, Osprey prop-rotor 38 ft.

Painting

Unless you go for a very early V-22, you're left with a Model T-type choice of paint-scheme: anything you like as long as it's Dark Ghost Gray uppersurfaces and Light Ghost Gray lower. I used Tamiya spray paints, which are very good but don't last long and, as I kept on having trouble with that top fuselage joint, I ran out at the crucial time. And who'd bought the last can at my local model-shop...? So, I got the airbrush out with the appropriate Revell colour and that was fine. But if I'd been on a different

schedule, I would, with hindsight, have waited to try out the relevant Mr Color offering in my airbrush.

As the kit hadn't exactly broken the bank, I'd got the Eduard masks for the canopy and wheels (though I didn't use the latter: life's too short to stuff a mushroom, as the lady said...) and I'm glad I did, as I think it would have been beyond me, otherwise. My previous encounter with Eduard masks left me with a delicate frosted effect on the

glasshouse of my Hasegawa Storch, so I approached this Kabuki stuff with some trepidation. It worked a treat, although I left myself with some ragged edges, which I was loath to use thinners on, for fear of damaging the surface.

Long ago I received free with an issue of SAMI a set of strange little 'brushlets' (unfortunately nearly all gone), the current equivalent of which is Microbrush disposable applicators. Though looking rather like miniature loo

brushes, they work amazingly accurately in small spaces. I used one of these to apply Rub 'n' Buff to a wheel hub.

The undercarriage is sturdy and represents the original well. And talking of wells, I made a bit of a nonsense of painting them white. With hindsight, I should have sprayed them before doing anything else, but I brushed them, and even with several coats got a streaky effect.



Photo Album





Final stages

The final stages of the build were refreshingly straightforward. I reduced the chances of my trashing the nose and upper rear fuselage antennas by pegging them on with brass wire. The undercarriage doors went on easily enough, but there's something wrong with the shape of the nose doors which

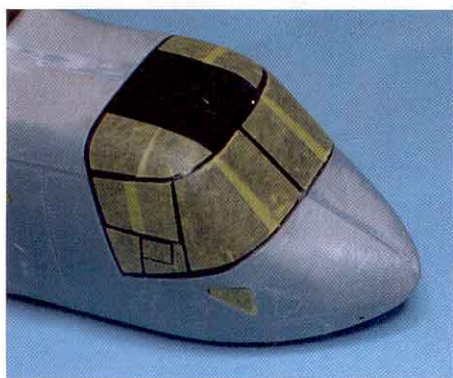
I can't pin down, as that's one area where Bill Norris's photos aren't specific enough.

The decals I decided to apply progressively, starting when only the wings were fitted, as, even once you have the nacelles on, manoeuvring the bird becomes a little tricky. I was delighted at how well the decals went

on to a coat of Klear. Nice and thin, didn't curl, easily manoeuvred into place, and settled down beautifully. I next fitted the nacelles and these fit very positively, so that you obtain the toe-in so obvious on the prototype. Applying the necessary paint halfway down the short intake in front of the turbine blades proved difficult until, by

trial and error, I made a mask in plasticard to fit snugly in the opening. The paintwork will be obscured by the intake covers but c'est la vie!

The Osprey has walkway areas on the upper fuselage marked by stripes and these are supplied. The more recent Ospreys have these on the main undercarriage doors, so they can be



"Kabuki tape worked a treat for masking the canopy, but left some ragged edges"



"With hindsight, I should have sprayed the white wheel wells before doing anything else but I brushed them and even with several coats got a streaky effect"



Basic airframe completed and painted



"I was delighted at how well the decals went on to a coat of Klear. Nice and thin, didn't curl, easily manoeuvred"



"The nacelles fit very positively, so that you obtain the toe-in so obvious on the prototype"

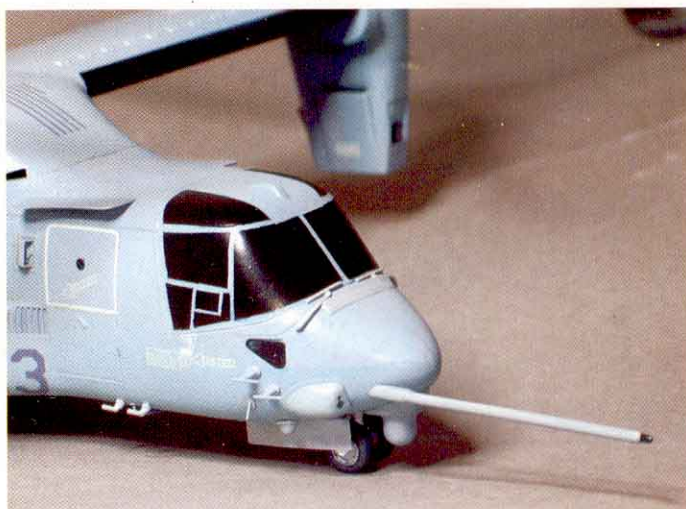


used as work platforms.

Having gone through a few paint iterations on the rotors, as I tried to disguise the differences of fit between parts, they eventually turned out reasonably and I was able to apply the warning stripes to the blades. Now, either Italeri didn't supply enough white decal strip to go all round the blades, or the stripes actually don't... I have no photos which show stripes on the rear of the blades. But as absence of evidence isn't evidence of absence I erred on the side of caution and made some white strip of my own: white paint on clear decal sheet. The nice

long pegs on the kit rotors drop nicely into holes in the nacelles. The instructions advise you to fix the shafts and rotor hubs into the nacelles with bushes, earlier in the build, but to avoid difficulties I ignored that.

One item I did leave off till the end, because I knew it would be knocked off, was the inflight refuelling probe. The kit part has an interesting and, I suspect, unprototypical, square section, so I replaced all but the stub, with the mounting lugs, with nested wire and aluminium tube. To make the finished object slightly more colourful, I put covers over the intakes (made from tissue paper painted red) and added Eduard pre-painted Remove before Flight tags.



Conclusion

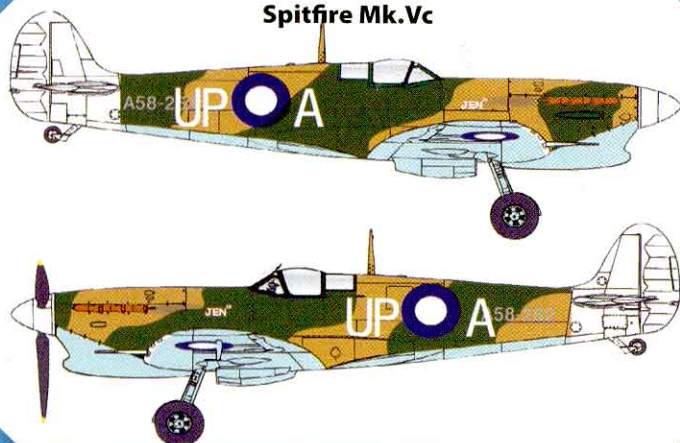
A nice build: most problems of my own making. A really good one to have on my shelf this: once I can find room for it... Something of a shock when I opened that *AirForces Monthly* in W H Smith and discovered that the V-22 was about to go into service. I'd got used to the convertiplane/tilt wing/tilt rotor being perpetually in development for my whole life. Literally, since Bell's concepts go back to the 1940s. You'll learn all about them from Norton's book. I wish the Osprey luck and the naysayers silence.

SAMI

WARBIRDS

New Releases

Spitfire Mk.Vc



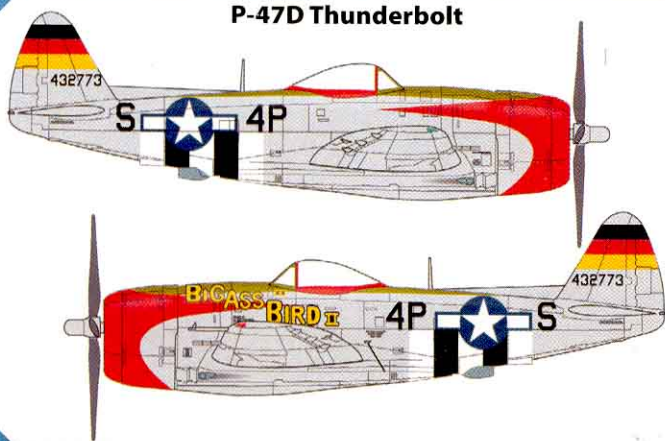
British Spitfire Mk.Vc UP-A "JEN III" 79th Squadron ~ A58-262, 1944. Spitfire Mk Vc with the code "UP-A", designated 79 Squadron and "A" the flight. About 250 of these variants were shipped to Australia for use by No. 1 Wing of the RAAF. The "C" in the name referred to the new four 20mm cannons that were carried on the wings. Scale 1:72. P/N DR50167

Messerschmitt Me262A-2a



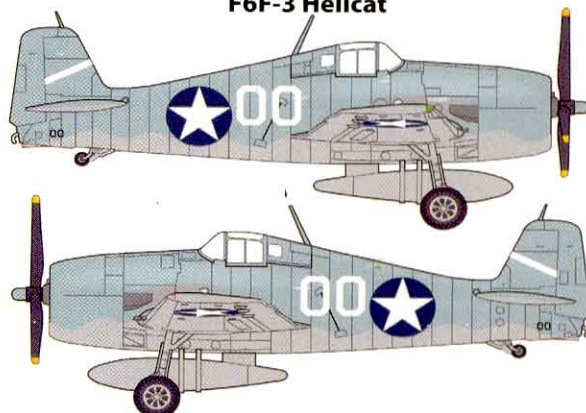
Messerschmitt Me262A-2a Bomber, 9K+BN, 5./KG(J) 51 "Edelweiss", Schwabisch Hall 1944. The most advanced fighter of the Second World War, but to few Messerschmitt Me262s were deployed by the Luftwaffe at too late a stage to affect the course of the air war over Europe. Scale 1:72. P/N DR50190

P-47D Thunderbolt



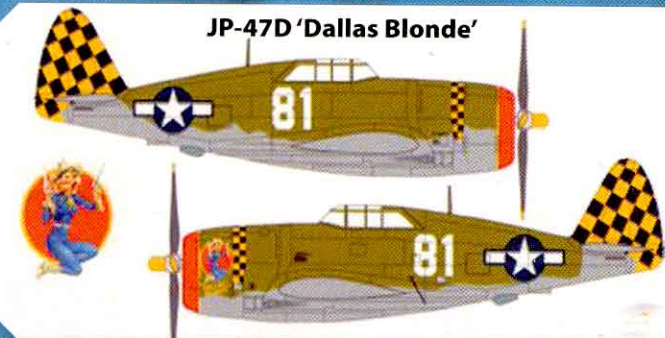
P-47D-40-RA Thunderbolt "Big Ass Bird II" Howard M. Park, 9th AF/406th FG/513th FS, 1944-45. The American Republic P-47 Thunderbolt, also known as the "Jug", was the largest single-engined fighter of its day. Scale 1:72. P/N DR50203

F6F-3 Hellcat



F6F-3 Hellcat, "White 00" CVAG-5 USS Yorktown, May 1943. Dragon Warbirds' brand new 1/72 scale replica is of Commander Flatley's "White 00" F6F-3 Hellcat as it appeared during his tour aboard the USS Yorktown. Scale 1:72. P/N DR50240

JP-47D 'Dallas Blonde'



JP-47D "Dallas Blonde", 319th FS 325th FG, "White 81" ~ Lt. Don Kearns. "Dallas Blonde" has the unique checkerboard livery on the engine cowling flaps and tail, plus the "Dallas Girl" artwork. Scale 1:72. P/N DR50274

B-17F-25 Flying Fortress 'The Duchess'



B-17F-25 Flying Fortress "The Duchess" 358th BS, 1944 ~ 1st B-17 in Dragon Warbirds. This B-17F-25 model is christened "The Duchess", as flown by the 358th Bomb Squadron of the USAF in 1944. Scale 1:144. P/N DR51003



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The internationally renowned Flying Legends airshow at the Imperial War Museum's Duxford home is one of the major events in the aviation enthusiast's year. One of the biggest and best opportunities to see beautifully restored military aircraft in their natural habitat, Flying Legends attracts thousands of visitors from all over the world to enjoy the displays, and to mingle with like-minded enthusiasts in a setting perfect for those with an interest in vintage military equipment.

As a modelling magazine, of course, we could not possibly spare the space to do justice to the exhibits, but from a modelling point of view, the show drew a vast number of our readers to come and look at the aircraft, to see in real life the effects of heat and exhaust on paint and metal, and to take satisfaction in the knowledge that the green, grey and blue machines sitting on the sun-baked flightline, or roaring and diving overhead in the bluest sky yet this Summer, look exactly like life-sized versions of the models we build!

The best we can hope to offer, then, is a brief glimpse of some of the places and the people we met, and wholeheartedly to recommend the event next year to one and all.

Thanks to all the many people who stopped to say hello and offer kind words and support over the weekend, to all our friends who helped out on a long busy watch, and to the IWM and all their staff for an absolutely cracking good show.

SAMI



IWM's Rebecca Wilson looked after the traders area, and is seen here enjoying a quick browse through SAM Publications' Datafile 9



People & Places

Flying Legends – Duxford

A look at the event from a modeller's point of view



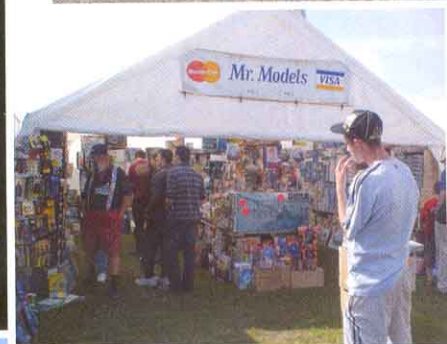
Kit Krazy is a fixture at most model shows, and their usual selection of quality used goods was on offer at Duxford



For some visitors the star of the show was the Special Hobby Blackburn Roc – seen here on Hannants' stand. Also debuting was the new Airfix Spitfire Mk I



The latest giant vacform from Heritage Aviation was on display...



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Heritage Aviation again. The modelling fraternity was well represented at Flying Legends this year



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Boulton Paul P.111/111a

Scale: 1/72 Kit No: 145
Price: £28.80 Decal Options: 2
Panel Lines: Recessed Status: New Tooling
Type: Resin 48, Plastic 10, Vacform Clear 4
Parts: Resin Vacform Clear
Manufacturer: CMR UK Importer: Hannants



Yellow Streak

No, it's not Thunderbird 4 – it's CMR's Boulton Paul P.111

You could build this kit in a weekend. Well, on second thoughts, to do a good job you need to be an expert modeller. You know the type. In slow motion, he leaps into a pit of rattlesnakes then calmly walks across it with the heart-aching grace of Dame Margot Fontaine, while performing martial arts moves that Bruce Lee would die for.

Me? Did I complete the kit in a weekend? Let's put it this way. That pit? I would have to be pushed into it. I would then disprove the theory that 'an object which starts life below the speed of light cannot accelerate past the speed of light' by doing so in the space of two feet from a standing start.

The kit looks innocent. A dinky little thing no bigger than your hand, with CMR's trademark minimal part count. But wait a minute, there's an etched set, produced in collaboration with

Eduard. Oh dear, fiddly stuff. And there's a choice of two different fuselages, two different ejector seats, two different undercarriage doors, two different canopies, and three different wings. One option even has four petit airbrakes.

Hmmm, decision time. Which combination of wings and fuselage to build? The decision time took out an entire weekend. I think the rattlesnakes died of boredom. The brain cell did a bit of overtime. If one was sneaky one could make a copy of the single fin and nose cone provided, and so be able to make both the P.111a and P.111b...

Construction

Those awfully nice chaps at CMR generally remove the pouring stubs from the major components, so all you need to do is trim and square the mating edges. I kicked off proceedings

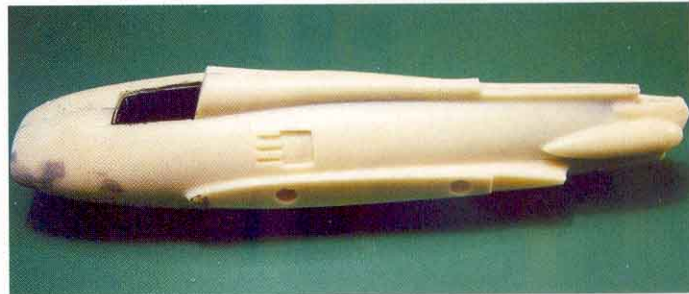
by doing this, and dry fitting the cockpit tub and nose cone.

The evil moment came. I had to start on the cockpit. The etched set is lovely. It encompasses sidewall detail, bits for the ejector seat, and a most excellent instrument panel. I used the latter in preference to the resin part. To show willing, I built the starboard sidewall and even glued it to the fuselage. However, given the tiny cockpit and its black colour scheme, not only did I leave off the miniscule parts, I didn't build the port sidewall, and only bothered sticking the seat belts on the ejector seats.

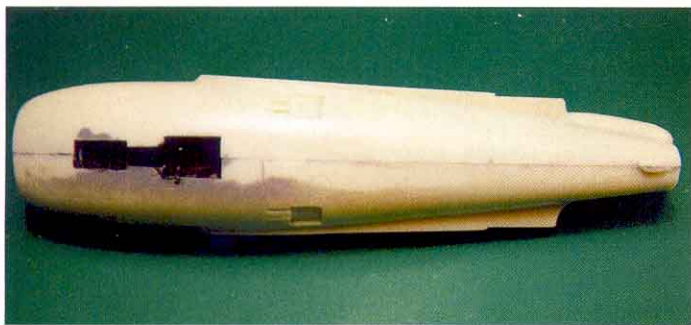
CMR provide two marks of ejector seat to cover the two different prototypes you can build. They are not the most brilliant of mouldings, and I couldn't see any difference between the two. To exacerbate matters, the instructions are none too clear on the



Eduard's superb etched part was used in place of the resin moulding



The fit of the fuselage halves was excellent



Moulding detail is generally good throughout

exact placement and orientation of some of the etched parts, and the Internet failed to deliver photos of sufficient quality to help me, so I sort of made it up.

The fuselage halves went together without drama, though I didn't align the tub properly so the nose wheel is offset to port. Oh well. The fit is excellent, but I elected not to fit the jet exhaust pipe at this stage.

I chose the stubbiest of the wings. I thought they were cute. Testosterone-filled male modellers read the last sentence with disbelief. Female modellers, seeing my tactile side, want to have my babies: photo and address to editor please. Such are the vagaries of resin, the port wing in particular did not fit very well, being 2mm short in the root and sitting too low. I removed the locating pins (I was surprised to find these – a first for CMR?) from the port wing, and trimmed them for the starboard wing.

Super glue gives off fumes. I know this due to the amount I slathered on to the airframe when joining the wings and fin to the fuselage. Copious amounts were used to extend the port wing root, and fill the gaps between all the parts. No idea what the fumes are doing to my body, but I daren't exhale too much in case the walls of my lungs touch and stick together.

Now onto the fiddly bits. Cutting the cockpit canopy from the backing sheet

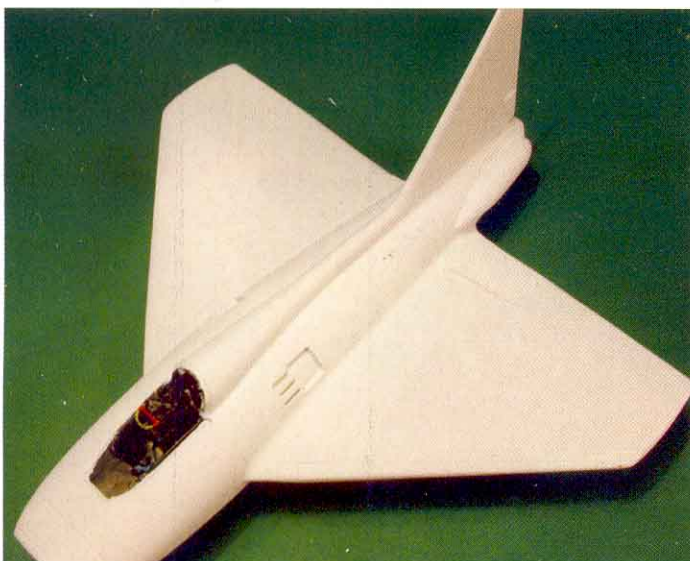


The resin ejector seat with belts in place

is the work of moments. Repeated gentle trimming and dry-fitting it to the cockpit opening takes an eternity. I added some stretched sprue and a bit of plastic to the back of the cockpit to help support the thin transparency.

Painting Stages

Finally on to painting. Firstly, a coat of Halfords white plastic primer, most important as we are going to paint the aircraft yellow. Faults, where I could be bothered, were fixed. A couple of coats of Citadel's Sunburst yellow did the trick. It looks about right. Then the



Wings need a little adjustment to fit correctly



Delta Design



In the late 1940s and early 1950s the British aircraft industry was engaged in many projects to confirm and develop the design ideas captured from the Germans at the end of the Second World War. As part of this activity the P.111 was built for the Air Ministry to specification E.27/46 by Boulton Paul Aircraft Ltd. to investigate the possibilities of the delta planform wing.

The P.111 was designed to be the smallest airframe capable of coupling an engine (Rolls-Royce Nene), an ejector seat (Martin Baker), and a delta wing. The latter could be fitted with a variety of extensions to investigate the aerodynamic effect of different tip profiles. The construction was all-metal with the exception of the wing extensions which were of glass-fibre.

The P.111 made its first flight on the 10 October 1950, at Boscombe Down with Squadron Leader Bob Smyth at the controls. Later test flying was carried out by the legendary test pilot, Alexander 'Ben' Gunn, who described the aeroplane as

General Characteristics

Crew: 1
Length: 26 ft 1 in (without nose probe)
Wingspan: 25 ft 8 in to 33 ft 6 in (with detachable wingtips)
Height: 12 ft 6 in
Powerplant: 1 × Rolls-Royce Nene R3N2
Maximum speed: 648 mph
Service ceiling: 35,000 ft
Rate of climb: 9,400 ft/min

'touchy' and 'like flying a razor's edge.' Because of this, and its bright colour scheme, the P.111 became known as the 'Yellow Peril.'

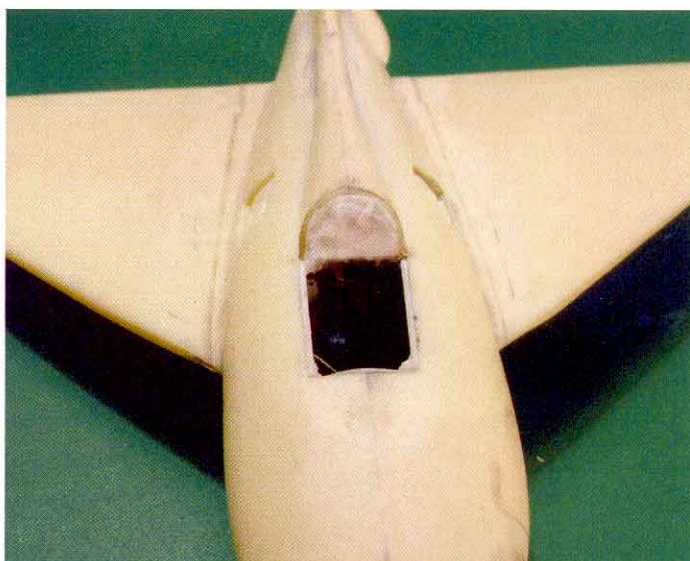
After a wheels-up landing the P.111 was modified to improve the flight characteristics, and reappeared in its new guise as the P.111A with much improved manners. After a last flight in

1958 it went to Cranfield as a training airframe. In 1985 it was transferred to the Midland Air Museum, Coventry, England, where it is still on public display.

The P.111 was followed by another delta-winged experimental aircraft, the P.120.

Recommended further reading:

Boulton Paul Aircraft since 1915, Alec Brew, Putnam, 1993, ISBN 0-85177-860-7



Beading added to the canopy aperture to support the delicate vacform



A closer look at some of the detail on the main undercarriage

intake lips were masked off and Citadel's Chaos black applied, only I hadn't rubbed down the primer, so the surface was rough allowing paint to creep under the masking tape. What fun I had over the next week trying to repair the damage.

I forgot all about CMR's decals. They are tremendously thin. They do not stand up to much abuse or even the normal handling of commercial quality decals. I tore the first one in half. With gentle handling the remainder went down well, except for the antiglare panel. It was far too big. I've never seen anything like this in the seven years I've been building CMR models, and no doubt, Petr Bouchar will be concerned as he takes a great pride in the quality of his products.

I thought about leaving the antiglare



panel, but it looked ridiculous without. I removed it and thought about masking the area but didn't like the idea of all those curves. Then I had a cunning plan. I cut up the antiglare panel decal, designed for the version I did not build, so that it fitted the

fuselage, masked off the straight bits to join them together then used Chaos Black to fill in the missing bits.

The only problem I had was putting on a coat of clear flat varnish. Could I get this right? Does it rain on an English bank holiday? Microflat

produced a mess. LifeColor flat produced a bigger mess. It was the bubbles, you see. You get them when you shake the bottles. Ronseal's clear flat acrylic varnish came to my partial rescue.

All the remaining dangly and easy to break off bits went on with no trouble, but the exhaust pipe was a tight fit. I posed the airbrakes open as I couldn't face trimming them to fit flush with the fuselage - plus it looks more interesting with them open. The internal colour of

the airbrakes is pure conjecture. The instruction diagram provides no clues, and neither did Internet searches. I decided to paint them black, like the wheel wells.

Another nice model of a lesser known British type from Petr and his gang. Thanks, boys. Keep them coming; we look forward to further projects from the prolific Czech masters.

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A comparative image to show the diminutive size of the prototype

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Diamond Studded Learjet

A study in metallic finishes by Frank Loch



Hasegawa's Box Art showing the attractive JAMDFS livery

U-36A Learjet 'JMSDF'

Scale: 1/48 Kit No: 09354

Type: Injection Moulded Plastic

Manufacturer: Hasegawa

UK Importer: Amerang



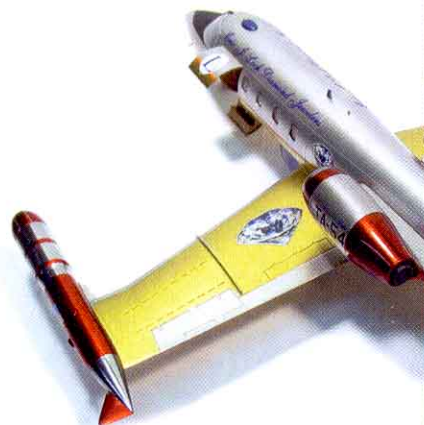
The author's son's jeweller's shop

About two years ago, I returned to model building after a 15-year hiatus. I have been building model aircraft since I was 6 years old in 1937 when my cousin gave me my first 'stick & tissue' model. During the war years I built a fleet of solid models carved from balsa wood and later pine, as the balsa supply was consumed for military purposes (life rafts we were told).

As you can see above, no mention of plastic. With the exception of helping some of my sons with plastic models, I never ventured in that direction until recently. I am presently working on my 29th and 30th builds. Early on in my Plastic Adventures, I became enamored with the possibilities of exploring various finishing techniques. As my experience in plastic modeling expanded, and I had completed a number of builds, I began to explore the possibilities of 'Plastic

Modeling as an Art Form.'

Most important to me at my present state of modeling is the requirement that *It Must Be Fun*, and hopefully bring excitement to others and myself. That said, my medium of choice seems to be Natural Metal Finishes. Much of my work to date has been with the Alclad 2 line of various metal shades and colors. Lately, I've begun exploring the additional possibilities of the SNJ line of Metalizers resurrected by Hawkeye's Hobbies LLC. The main difference in the appearance of SNJ compared with Alclad (as I see it), is that the SNJ colors seem to have a deeper (in visual surface depth), warmer, metallic luster, while the Alclads, have a cooler, shinier, even brighter look to many of the shades.



The Cabin Floor undergoing 'treatment'





Diamond Studded Learjet

A study in metallic finishes by Frank Loch



Hasegawa's Box Art showing the attractive JAMDFS livery

U-36A Learjet JMSDF
Scale: 1/48 Kit No: 09354
Type: Injection Moulded Plastic
Manufacturer: Hasegawa
UK Importer: Amerang



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Construction

I decided to start on the internal construction, and in this regard, the cabin floor looked like a good piece to begin with. It was while working with this piece that an unexpectedly serendipitous result occurred. As I was testing this piece of Hasegawa white plastic for compatibility with Alclad 2 Titanium Gold lacquer paint, I discovered that this set of sprues was not compatible (at least unprimed). The surface bloomed into a superbly roughened or textured floor surface. I thought this looked great for a Non-Slip cabin floor. I then simply over sprayed it again with a compatible SNJ Gold color! This result pointed out to me that further use of Alclad colors (without primer) was not a good idea for this kit and should probably be avoided.

Proceeding further, I then sprayed the cabin interior sides with SNJ Bronze



Left fuselage side sprayed with SNJ Bronze



The windows masked prior to blending the two Platinum areas together



Fuselage masked and fin/rudder sprayed with Model Master Glossy Black Enamel



Fin/rudder sprayed with Alclad 2 Chrome, and over sprayed with Gunze Clear Red



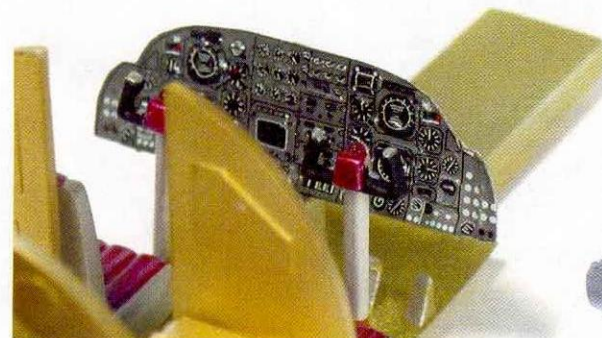
Completed and Decal'd Fuselage Assembly

with SNJ Bronze, and Platinum for the seats and joysticks. The seats were then upholstered with Model Master Purple Pearl Acrylic. The instrument panel was sprayed with SNJ gunmetal, and overlaid with the kit decal. This

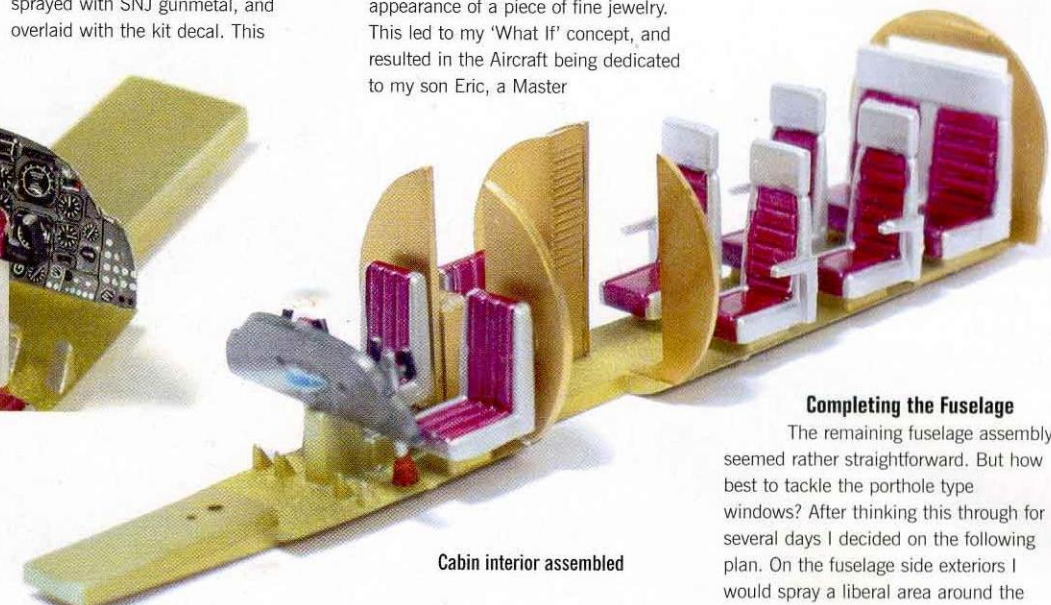
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Jeweler and owner/operator of Eric J. Loch Diamond Jewelers! I thought it would be nice if he had his own Learjet!



with a new Iwata Eclipse 3500 SBS (side feed) at pressures between 18 and 35 psi (I was experimenting to find the best pressure point, later determined to be about 18 psi for the SNJ paints). Individual cabin parts were sprayed



Cabin interior assembled

Completing the Fuselage

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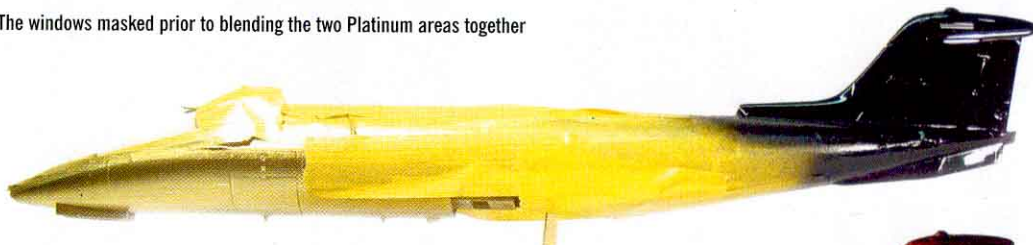
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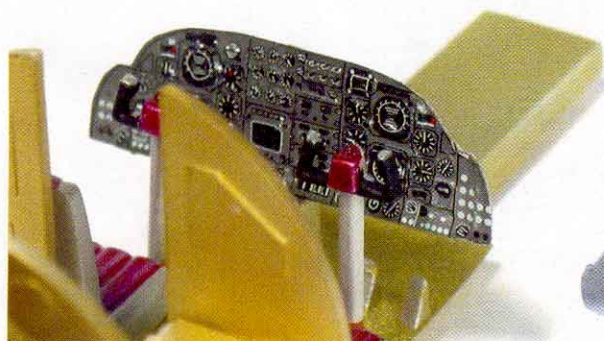
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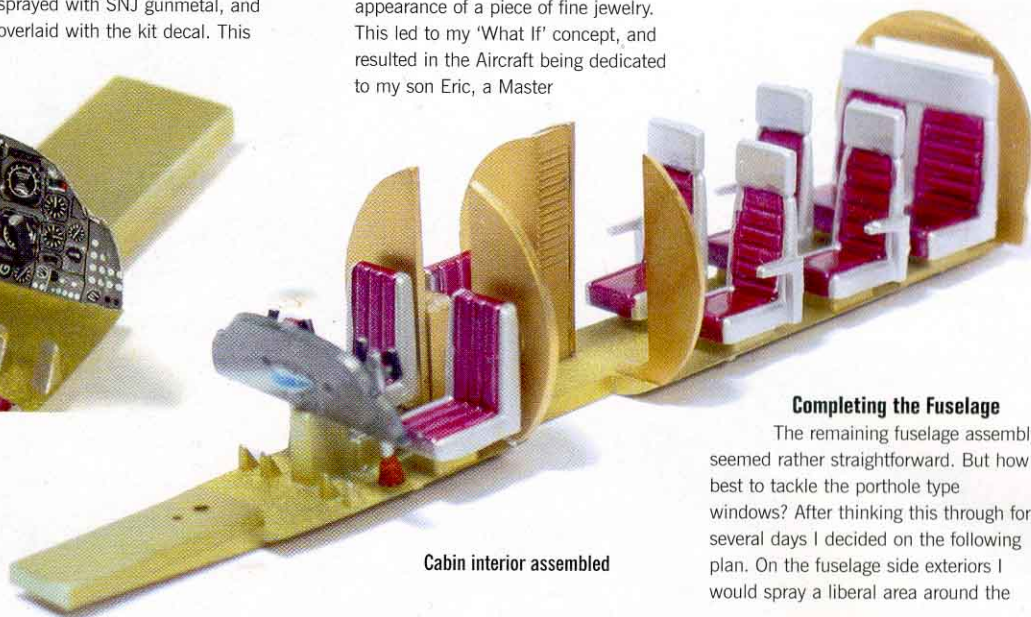
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Model Master Glossy Black Enamel applied over polished plastic surfaces

remove any excess with a 2"x2" patch of soft cloth. I let this surface dry untouched then for at least 4 hours. Often I let it rest overnight, after which masking tape can be safely applied. I made no attempt here to emphasize the panel lines, as those are nicely recessed on the kit surfaces and did not seem to need any additional treatment.

I had previously been experimenting with clear acrylic colors applied over Metalizers. This combination produced some very spectacular Metallic looking colors, and in particular superb red metallic shades. I continued to develop this technique and discovered that Alclad 2 Chrome, applied over a very Glossy Black finish (spectacular in itself) produced a superb metallic red. Furthermore, this metallic red color could be controlled from bright red to various shades of maroon, by varying the amount of Alclad 2 Chrome sprayed on to the Glossy Black undercoat.

Gilding the Lear

The Platinum upper half of the fuselage was then masked off including the fin/rudder. SNJ Gold was applied to this lower half in the same manner as the platinum on the upper half. Masking was then removed from the fin/rudder area and the balance of the fuselage masked off leaving only the fin/rudder exposed. Model Master Glossy Black Enamel was then applied to the fin/rudder. The MM GBE was thinned to 1/1 or a bit more with lacquer thinner. This mix was then airbrushed with an Aztec 480 at 18 psi



Alclad 2 Chrome applied



Gunze Clear Red applied



SNJ Platinum and Decals applied

windows (prior to installation) with SMJ Platinum then attach the windows internally to the fuselage sides. Next assemble the fuselage sides. Then apply Tamiya masking tape over the windows externally, covering a fairly large platinum area around the windows. I could airbrush the upper fuselage with SNJ platinum then peel off the masking tape and replace it over the portholes with a piece not much larger than the windows themselves. This left a zone of SNJ Platinum that could be lightly re-airbrushed and powder polished to blend seamlessly with the outer zone first laid down.. This worked out quite successfully.

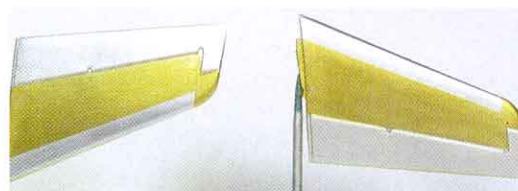
My method for applying SMJ metalizers is to Airbrush 4 light coats separated by ten minutes drying time. Then I wait 30 minutes prior to applying the SNJ metalizer powder on to the slightly tacky surface lightly with a Q tip. I then polish this powder and





using the white 0.060 nozzle. This Glossy Black enamel must be applied until it develops a 'wet' look. Then when it dries, it will still look 'wet' -- a superb glossy finish when applied to properly polished plastic parts. It's important to let the GBE dry several days, perhaps even 4-5 days depending on how heavily it was applied, particularly if you wish to mask on it safely without tape tracks.

Next the Alclad 2 Chrome was applied. I used the Aztec 480 and the tan 0.030 nozzle for this at 15-18 psi. After 24-48 hours drying time the Clear Red Acrylic was applied. In this case I used Gunze Clear Red



Finished Stabilizers with SNJ Gold, Aluminum, and Platinum

Alclad 2 Chrome being applied



Completed Wing Assembly upper surface



acrylic. I applied this with the same set-up as the GBE. However this was thinned about 1/1 with alcohol.

The Decals (Custom made for me by Walt Holms) were now applied to the fuselage (Script, Hallmark, and Diamonds). The kit red stripe decals (three pieces each side) were also applied at this time. All responded nicely to Micro Sol/Set.

Wings and Pods

The wing assembly was then airbrushed with SNJ Gold. Again four light coats separated by 10 minutes dry time. The Iwata Eclipse

wings were masked off leaving the pods ready for the MM Glossy Black Enamel applied as before.

Next, the pods were masked a bit for a particular decorative pattern, and the Gunze Clear Red was applied to the pods in the same manner as on the fin/rudder.

Final Assemblies

After sanding the join seam, and polishing the surface, the engines were ready for finishing. First the MM Glossy Black Enamel was applied as above and given 2 days to dry. Next the Alclad Chrome was applied over the Black, and allowed 2 more days dry time. This was then masked and Gunze Clear Red applied. This was given 2 more drying days. The Red was masked and then the SNJ Platinum was applied (and powder polished). At this point I added the exhaust cones into the rear cavity of the engines, and applied the decals.

The stabilizers were sprayed overall with SNJ Gold and powder polished. After an overnight dry time, the center gold section was masked and leading

edge panels and elevators were then sprayed with SNJ Aluminum (leading edge panels) and Platinum (elevators). These were also powder polished prior to removing the masking tape.

I attached the wing to the fuselage with CA glue, and then assembled a simple styrofoam jig to hold the fuselage and wings and insure that the stabilizer alignment was true. The engines were then glued in place with CA, making sure that the +15 degree angle was observed on each side. Undercarriage and additional details were added, and the model was complete.

Conclusion

With the exception of the custom printed decals, this was essentially an 'Out Of The Box' build. This well detailed and refined Hasegawa kit assembled easily and could be recommended for virtually all skill levels.

For me it was a very enjoyable build.

SAMI



Letter of the Month Competition

Revell UK are offering a monthly prize for Readers' Letter of the Month.



Congratulations to this month's winner of the Revell 'Letter of the Month Competition'. Chris receives a selection of paints from the new Revell Aqua Color range.

Something of Interest

LETTER OF THE MONTH

I was talking to my Dad not too long ago about his Naval career and asked if he had any photos of Gannets and he showed me the ones that I have attached. The photos are of Gannets and Seahawks operating from HMS Centaur, the photos were taken between 1957 and 1959 and include a nice shot of a Gannet in flight, and one coming in for a belly landing and destroying the net. I don't know if they will be any use to you, but I understand you or one of your staff likes Gannets anyway.

All the best.

Chris
by email

Ed Note: Thanks for the pictures – always a pleasure. Can you drop us a line with a mailing address to send your paints to.



Send Your Letters To...

Please send your comments, questions and feedback to the editorial address, clearly marked 'Feedback'.

Readers' Feedback

The views expressed in this column are not necessarily those of SAM Publications. Readers' address details must be supplied but we will publish them only if the letter is considered to be a request for assistance or further correspondence from the readership.

This page will be used to publish letters we receive from the readership that relate to articles previously published in this magazine. These letters will add to, update or revise such articles.

Some Talk of Alexander...

I just thought this information, sourced from Belfast The Story of Short's Big Lifter by Molly O'Loughlin White, may be of interest following the reader's letter with regard to the Magna Models kit build; the article titled 'A Belfast by any other name...'

The names of the Belfasts were:

- XR362 *Samson* (G-BEPE)
- XR363 *Goliath* (G-OHCA)
- XR364 *Pallas*
- XR365 *Hector* (G-HLFT)
- XR366 *Atlas*
- XR367 *Heracles* (G-BFYU)
- XR368 *Theseus* (G-BEPS)
- XR369 *Spartacus* (G-BEPL)
- XR370 *Ajax*
- XR371 *Encaladas*

Regards

Gary Anslow
by email

Back to Basics

I have travelled a common road in relation to building model kits, starting at the age of 6 or so with Airfix and Frog in 1/72. Had a break between leaving high school and turning about 30, whereupon I recommenced modelling in 1/48.

Now, at the age of 40, I build, I believe, fairly high standard kits, certainly comparable to those I see in various model shows. This last weekend, however, I experienced something of a modelling epiphany.

I had set aside Saturday to work a kit or two towards completion; I had several to choose from:

- 1/32 Revell Tornado – wrestling with the Paragon flaps/slats set here
- 1/48 Hasegawa Hurricane – fitting the excellent Eduard Flaps to it – all those tiny ribs!
- 1/48 Dynavactor Sea Vixen – Awaiting its canopy and final paint job
- 1/48 Mustang – awaiting final painting
- 1/48 P-47 – likewise awaiting a bare-metal paint job

And several others in various states of construction.

Whilst fishing out the Hurricane, I came across my Hasegawa Mk IX Spitfire...

I thought, 'Let's revisit my youth, and see if I can build this in a day, straight out of the box.'

The kit was duly commenced, no extras were added, the cockpit was assembled, I discovered I had run out of RAF Cockpit Green, so horror of horrors, Luftwaffe light green was used instead – you know what? It looks great. No additions, no brass instruments, shock – no brass/tape/



champagne foil seatbelts either.

Construction continued apace. The kit was still unfinished at close of play on Saturday, but the decals were eventually applied Monday evening whilst keeping one eye on Wimbledon! This Spitfire is now resplendent in its kit-supplied decals, just awaiting it's aerial to complete it fully. The dimensions, especially the length, of this kit have in the past been a hot topic in modelling publications and Internet sites, and I had even purchased a nose correction kit for it – I didn't use it, and it looks exactly like a Mk IX Spit to me, so who cares?

What an absolutely liberating and enjoyable experience, I felt like a schoolboy once again, completing a series 1 or 2 Airfix kit after school.

I guess my point is that I seem to get bogged down in the maelstrom of research/brass/ resin accessories/ after market decals etc, and lose sight of the reason I got involved in the hobby in the first place – to enjoy building kits!

I have decided that I will build more kits as 'out of the box projects,' my Spitfire looks just as good as my brass/Xtracolor enhanced Typhoons, and took me a sixth of the time to complete.

Regards.

Simon Lovatt
by email



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Luxury Kits



more on www.eduard.com

IT'S TIME FOR A CELEBRATION AS...

HLJ marks its 10th year!

That's right! It's been a full ten years since HLJ was first incorporated back in January of 1997. We're celebrating our first decade of dedicated customer service with a series of special promotions and incentives. These can only be found on the HLJ website, so type in www.hlj.com to check 'em out! Birthday hats are optional!

Now that's what I call a happy birthday!

Scott T. Hards,
President of
HobbyLink Japan

**Happy
10th
Anniversary
HobbyLink
Japan**
www.hlj.com

Ha-pee birth-day mister president...

NOT Scott T. Hards, President of HobbyLink Japan →

Avro Vulcan – Britain's Famous Delta-Wing V-Bomber

BOOK OF THE MONTH

This is another absolute gem from Aerofax and a timely issue with the 25th anniversary of the Black Buck raids very much to the fore, and the anticipation of XH558 getting back into the air. The Vulcan, as everyone knows was one of three aircraft designed to form a part of Britain's V-bomber fleet to carry the country's independent nuclear deterrent alongside the Valiant and Victor, and remains one of the most iconic aircraft to have been designed and built in the UK in the post war years.

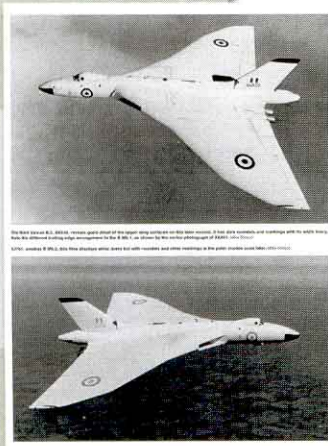
Ironically, however, the Vulcan did not drop a bomb in anger on its designed target area, but bowed out of RAF service with the longest

TECHNICAL DATA

Authors: Phil Butler and Tony Buttler
Publisher: AEROFAX Midland Publishing
ISBN: 9781857802566
Format: Softback, 176 pages Price: £19.99

bombing raids in history against the Argentine Garrison on the Falkland Islands in April 1982. This latest addition to the highly successful Aerofax series contains all the information the enthusiast and modeller will need on the subject, backed with informative text, excellent black and white images, and a superb colour gallery, plus a comprehensive listing of aircraft histories.

The book begins with the Vulcan's genealogy, through paper designs, scale models and test vehicles, through to the production machines and their variants, plus a look at aircraft retained for trails



Avro Vulcan

Britain's Famous Delta-Wing V-Bomber



Phil Butler and Tony Buttler

Aerofax

work and engine test-beds. This is a superb book and one that deserves its place on any self respecting enthusiast's bookshelf. Five Star!

AE

SAMI

Arado Ar 234 Blitz

TECHNICAL DATA

Author: Marek J. Murawski Price: £TBA
Publisher: Kagero ISBN: 9788360445662
Format: Softback, 96 pages

With Revell's re-issue of the 1/48 tooling as a nightfighter, and MDC's 1/32 kit just around the corner, there is definitely room on the shelf for a good informative title on the Ar 234, and with Kagero's growing reputation as a specialist modeller's publisher, this is going to be the book that many will turn to.

Neither will they be disappointed. With the first two thirds given over to an all-English historical text, accompanied by plenty of excellent photographs, the final section is the icing on the cake, consisting as it does largely of original technical drawings, diagrams and illustrations from a period factory manual.



A sheet of wheel and canopy masks in 1/48 is included. Simply superb.

Barry Brine

SAMI

Diaerama – Issue 1

TECHNICAL DATA

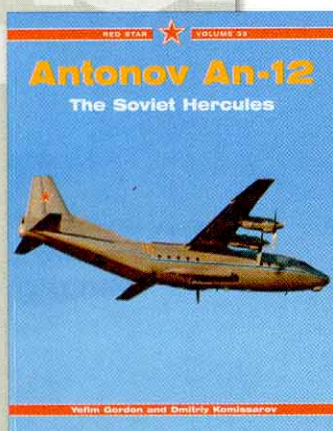
Edited by Tony Clayton
Contact the Editor on 01691 778613, or email
diaeramamag@aol.com

Diaerama is an interesting and unique new magazine aimed at presenting the modeller with information and assistance in the production of dioramas. There are so many new products available to aid the modeller interested in displaying aircraft in a scenic setting, but much of it has been overlooked or ignored by the aviation press, as many regard it as being aimed at military modellers.

This initial production is unbound, but contained in a plastic spine, having been home produced on a superior-quality laser printer (I think) and it is hoped that in time a more mainstream presentation will be possible. This aside, it is well presented, and packed with diagrams, plans, full colour images and a great deal of useful information for interested parties.

GH

SAMI



Red Star Volume 33

Antonov An-12 – The Soviet Hercules

There is always a frantic dash for the review copy when the latest Red Star volume arrives. Being inclined towards transport and training aircraft myself, I was pleased to lay my hands on this one, as the Tupolevs and Antonovs of this world are a mystery to me, and one that I am keen to penetrate.

Yefim Gordon and his colleagues have come up trumps again. This is a well-

balanced and useful book that tells you all you need to know about the aircraft to cater to your basic modelling needs, without going to deeply beneath the surface – although given the breadth of the subject matter this is no mean

TECHNICAL DATA

Author: Yefim Gordon and Dimitriy Komissarov
Publisher: Midland ISBN: 9781857802535
Format: Softback, 128 pages
Price: £19.99

achievement in a book of this size.

Plenty of photographs, many in colour, and a set of line drawings depicting the different variants add up to as near as complete a reference as you could hope for, and for those who worry about such things – it's the right size and shape too!

Jack Trent

SAMI



Schlachtflieger

TECHNICAL DATA

Author: J. Richard Smith, Chris Goss, Martin Pegg,
Andrew Arthy, Nick Beale & Robert Forsyth
Publisher: Midland ISBN: 9781857802740
Format: Softback, 96 pp Price: £16.99

Continuing the Classic Colours series, Schlachtflieger has managed to find an area of Luftwaffe operations that has remained relatively undisturbed, and as a consequence – to the mere dabbler in this particular realm – there are one or two eye-openers.

Needless to say, all the usual suspects are present, and Fw 190s and Stukas feature large in these pages, but aircraft like the He 50 and the Hs 123 also get a good hearing, and one item of particular interest to many will be the belligerent little Bucker Bu 181, depicted on the cover, which machines were used to equip Panzerjagdstaffeln, armed with anti-tank rockets under the



wings in the last desperate days of the war.

If you collect the series, you will not be disappointed. If you do not, then now might be good time to start.

GH

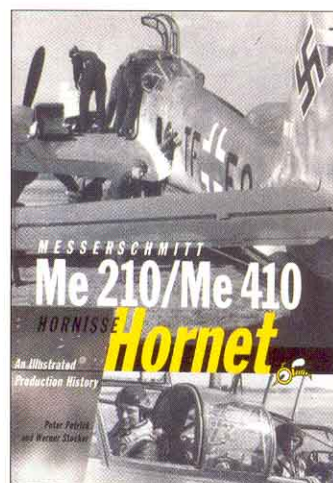
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An Illustrated Production History Me 210/410 Hornet

TECHNICAL DATA

Author: Peter Petrick & Werner Stocker
Publisher: Ian Allan Publishing
ISBN: 9781857802719
Format: Hardback Price: £29.99

Planned as a further development of the Bf 110, the Me 210 proved extremely troublesome, suffering from in-flight instability and landing-gear problems. However, the sleek Daimler-Benz-powered Me 410 was much more successful, being manufactured in large



The book contains a comprehensive production log as well as diagrams of proposals for other Me 410 variants. This is yet another essential purchase for any Luftwaffe enthusiast's collection.

AE

SAMI

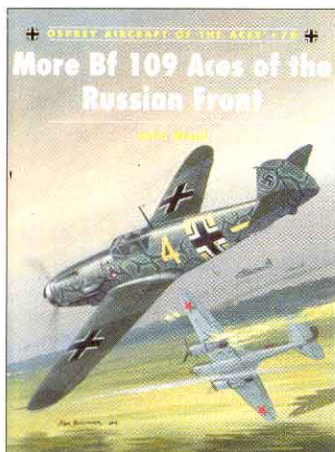
More Bf 109 Aces of the Russian Front

TECHNICAL DATA

Author: John Weal Price: £12.99
Publisher: Osprey ISBN: 9781846031779
Format: Softback, 96 pages

This is the follow on from Osprey's previous volume, which dealt with some of the highest scoring aces of the conflict. Ironical that pilots scoring between 50 and 100 victories should be consigned to a second volume, almost as also-rans, but such was the extreme nature of the conflict. Fortunately the book does not regard them as being in any way second rate, and is a worthy successor to the first part in its representation of the men and machines involved.

Following Osprey's usual format, the book is well written and beautifully illustrated and a welcome addition to the



Osprey library.
GH

SAMI



numbers with more than a hundred variants and sub-variants, which this book enables the you to easily distinguish and identify. Packed with black and white images, interspersed with colour, this book ably handles the subject in an interesting and informative way, and the authors have matched colour profile drawings to actual aircraft photographs for added detailing.



Italeri 1:48 Fairchild Republic A-10A Thunderbolt II

CD-ROM: TECHNICAL DATA

WEEAC Navigator Series Price: £5.95
Published By: WEEAC
Contact: www.weeac.co.uk

This is a nice intuitive CD-Rom which auto-runs into a well structured navigator page from where you can access five different area of the CD. The images are top quality and the instructions on the various pages are highly informative and easy to understand, taking you step-by-step through the process of building, finishing, painting and weathering the 'Warthog'. The Gallery section also



includes models of two other aircraft that carried the 'Thunderbolt' name, these being the P-47 and the Russian 'Sturmovik'. If you want to know all there is more on How to build a Hog, then this has to be the best place to start!

AE

SAMI

Air Battles 02

P-51B/C Mustangs Over the Third Reich

TECHNICAL DATA

Author: Tomasz Szlagor Price: £7.95
Publisher: Kagero ISBN: 9788360445600
Format: Softback, 80 pages

Following the arrival of P-40s over the Mediterranean, Kagero have continued the series with another first rate aviation story. Differing from run-of-the-mill reference books in its anecdotal format, this volume is much more than a collection of useful pictures and text - it is a darned good read.

So much of the Mustang's early success has been over-shadowed by the subsequent activities of the bubble-headed fraternity that it is a pleasure to see this book in print, and certainly a pleasure to read it. Hopefully Kagero will let their imaginations range far and wide over the various theatres of the Second World War with this series and bring us some more fascinating stories.



Also included is a 1/48 masking set, shrink-wrapped securely inside the book. Highly recommended.

Barry Brine

SAMI

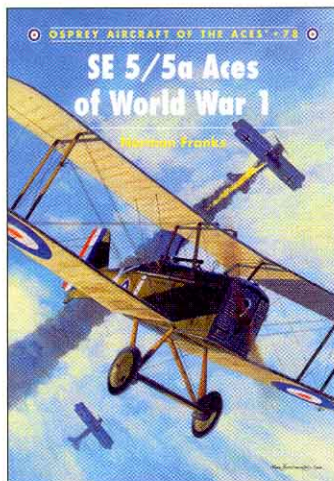
Aircraft of the Aces 78

SE 5/5a Aces of World War I

TECHNICAL DATA

Author: Norman Franks Price: £12.99
Illustrator: Harry Dempsey
Publisher: Osprey ISBN: 9781846031809
Format: Softback, 96 pages

A veritable feast for the WWI aficionado from Osprey with this first rate title. Here we have the British Royal Aircraft Factory SE 5/5a which was, along with the Sopwith Camel, the major British fighting scout of the last 18 months of the war in France. It equipped several notable squadrons, the first being No 56 in April 1917. This unit became famous for the number of aces it had among its pilots,



including Albert Ball, James McCudden, Geoffrey Bowman, Richard Maybery, Leonard Barlow, Hank Burden and Cyril Crowe. In all, 26 aces flew the aircraft with No 56 Squadron alone.

A number of Victoria Cross winners also flew SE 5/5as, namely Ball, Mannock, McCudden, Beauchamp Proctor and Bishop. Among the aces, no fewer than 20 scored more than 20 victories. In all, there were almost 100 SE 5/5a aces, and a large number of them are profiled in this excellent book. Supporting the text are more than 110 photographs, 37 stunning Harry Dempsey colour artworks and detailed appendices listing every pilot who 'made ace' on the SE 5/5a. Totally recommended.

AE

SAMI

Aircraft of the Aces 77

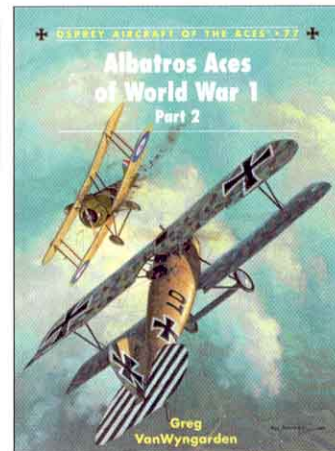
Albatros Aces of World War 1 Part 2

TECHNICAL DATA

Author: Greg Van Wyngarden Price: 12.99
Illustrator: Harry Dempsey
Publisher: Osprey ISBN: 9781846031793
Format: Softback, 96 pages

Greg Van Wyngarden continues his Albatros saga with this much anticipated follow on work to his AOTA 32. From September 1916 until late 1918, biplanes from the Albatros firm formed the primary equipment of Germany's fighter forces. Starting with the D.I of 1916, these aircraft underwent a continuous programme of development and production to the D.Va of late 1917. Albatros fighters reached their zenith of deadly efficiency in the spring of 1917, when the Albatros D.III took a heavy toll of Allied aircraft.

Nearly every one of the 81 *Jagdstaffeln*, or fighter squadrons, operated one or more types of highly decorated Albatros aircraft at some point in their history. This book provides a look at the design and production of the series and also details the careers of



some of the war's best known and lesser-known aces. The exploits of such luminaries as Ernst Udet, Max Müller, Karl-Emil Schäfer and Julius Buckler are recounted through their own first-hand accounts, rare archival photography and again backed up with Harry Dempsey's superb colour artwork.

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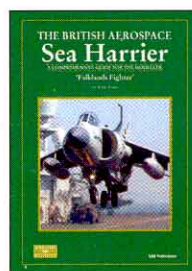
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Modellers Datafile 11

Sea Harrier Out Now!

See page 736 for details

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USAF F-4 Phantoms Part 1

CD-ROM: TECHNICAL DATA

Published By: Aero Research Co
Catalogue No. 1004
Price: US\$ 11.95 plus \$4 shipping to UK
Contact: aeromodel@aol.com

This is the second dedicated F-4 Phantom CD from Aero Research, but the first entirely made up of the USAF versions of the F-4C, F-4D, F-4E, F-4G, RF-4C and QF-4E/G. The disc contains some 175 images, which vary in quality from snapshots to posed flightline photography, with the subject matter varies from early Phantoms to the Phinal Pharewell. Amongst the photographs are some real gems from the Vietnam War, and most pleasing to see is that a few early Wild Weasel F-4C's are also included.

The wide variety of camouflage colours is another real plus on this CD,



from the early ADC Greys, through SEA Lizard to the later Hill Grey, interspersed with a liberal smattering of other unusual variations thrown in for good measure. As usual with this series, captions are minimal, with only the aircraft serial number, unit and the location of the photograph included. However, in this case that's all you need!

AE

SAMI

USN/USMC A-4 Skyhawks Part 1

CD-ROM: TECHNICAL DATA

Published By: Aero Research Co
Catalogue No. 1003
Price: US\$ 11.95 plus \$4 shipping to UK
Contact: aeromodel@aol.com

This is a spectacular CD dedicated to The Scooter in all its glory with plenty to keep any enthusiast happy for years! With over 170 images this disc does not disappoint, as the whole gamut of early flamboyant and later toned down colours are present. Plus there are the Aggressor schemes to consider, and these are bound to have any enthusiast heading for the nearest Skyhawk kit to reproduce some of these wonderful colour variations!

You also get one or two very unusual images such as the VA-36 A-4C taken aboard the USS Saratoga which appears to have parts of its upper surfaces spray painted with criss-cross blue lines! All of



the variants are represented including the two-seaters, and again the photographic quality varies from snapshots to posed flightline, but with so much to choose from it's hard to be picky.

Contact them at: 6468 Valley Wood Drive, Reno, Nevada, NV 89523-1265 USA. Payments are via credit card or PayPal.

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Modellers Datafile 10

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CLUBS

Bridlington & Wolds Scale Model Club

Whether experienced or beginner, or returning to the 'dark side', a warm welcome is assured. We meet on the second Monday of each month at The Parade Hotel, 26 Cliff Street, Bridlington (next to Woolworth's) from 7.30 pm onwards. For more details contact Kevin Dolman ☎ 01377 255594

Aberystwyth Scale Model Club

Meets on the first Tuesday of the month at JD Wetherspoon, Yr Hen Orsaf, Aberystwyth, Ceredigion, Cymru at 7.30 onwards. Everyone welcome. All Scales. No Trains! No Flyers! Contact Fiona ☎ panzergrill@mhops2.wanadoo.co.uk

IPMS Farnborough

Meets second Monday of each month at the Railway Enthusiasts' Club, Hawley Lane, start 7.30 p.m. Large clubroom with real ale bar, big, friendly crowd, all ages/abilities welcome. Pete Readman ☎ 0118 978 2910 ☎ www.ipms-farnborough.co.uk

IPMS Ipswich

Meets on the fourth Tuesday of each month at Ransomes Sport and Social Club, Sidegate Avenue, Ipswich, Suffolk IP4 4JJ. All welcome, all modelling standards. Contact Kev Baxter ☎ 01473 832163 or Ray Challoner ☎ 01473 721850 ☎ www.ipms-ipswich.org.uk

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Maintenance Manuals, AP2210 C-R, F3-U16. Ideal for enthusiastic Meteor modellers. Hampshire ☎ 01276 504328

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(new items since added). For list send SAE to: T. Pilkington ☎ 37 The banks Wallasey Merseyside CH45 3NS

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Windsock Datafiles

14 RAF BE2e; 15 Morane-Saulnier Type L; 28 Avro 504K; 40 RAF S.E.5; 38 Airco D.H.10; 44 Bristol Scouts; 46 Sopwith Snipe; 52 Bristol M.1; 68 Nieuport 10-12; 75 Ago C1. £4.50 each ☎ Andy_swift@tiscali.co.uk

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Small selection of my work. SAE for list to G McWhirter ☎ 90 York Crescent, Newton Hall, Durham City DH1 5PT

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Serious offers. Also FROG exotica. Roy Dagger ☎ 01493 602887

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'JV44 The Galland Circus'

Classic Publications, signed by authors. Very good condition. Substantial offers to include p&p to ☎ prfowkes@btinternet.com

Large collection needs sorting out

1/72, 1/48, Tamiya, Monogram, Hasegawa, Fujimi, etc. Aircraft, military, naval. For lists ☎ thomasheath.159@btinternet.com or send SAE to ☎ 101 Cheriton Road, Folkestone, Kent

Aircraft kits, all makes/scales

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Large selection of kits

All 1/72 scale, some rare items e.g. Revell 'Sally' and 'Helen' models. For list please contact Dennis Walton ☎ 49 Windward Road, East Kilbride G75 8NR, ☎ 01355 238200 ☎ dennisswalton@yahoo.co.uk

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MPM XP55 Curtis Ascender (upgrade) 1/72, Monogram or Revell F7F-3 Tigercat. Top prices paid. Ron Parfitt ☎ 01430 424492

Leading Edge sheet #7223

for SAAF F-86 Sabre, even if only spare for the camouflaged option. Can anyone help? Marco Zenati ☎ Via Gasparini nr. 11/1, 24125-Bergamo, Italy ☎ saffo@tiscalinet.it

Aero-Detail No 27 Spitfire VI to XVI

Also looking for 1/48 scale decal sheets Techmod 48029 Spitfire Vb and 48064 Spitfire IXs. Willing to pay a fair price or swap for kit decals I have for sale. John Collier ☎ 23 Stonehill Close, Ranskill, Retford, Notts DN22 8NG

Contrail Saro Lerwick

PH Models Blackburn Botha. Good prices paid ☎ 07985 512471 ☎ neil_castley@yahoo.co.uk

Aeroguide on the EAP

(Experimental Aircraft Programme), published by Linewright in mid/late 1980s. Contact Guy Henderson ☎ 01475 726221 or 07919 497769 ☎ guy.henderson@clydeport.co.uk

SuperScale decal sheets 72-141

and 72-688 and Delta kit of Caproni Campini N1. Roy Dagger ☎ 01493 602887

Copper State Models photo-etched

1/48 nuts and bolts (CSM 106). Cutaway close-ups of WWI SPAD XIII: skeletal frame layout of forward fuselage section, GA drawings of oil, petrol, air pressure systems etc. J McSkimming ☎ 96 Coronation Road, Drongan, Ayr KA6 7DD

Help! I am addicted to resin

and fiddly bits of etched brass. Can you help me? I will buy your unwanted short-run kits and brass/resin detail parts. Particularly wanted USAF/USN and RAF/FAA, but anything considered if the price is right. Nev Fadries ☎ 6 Deby Street, Mossley, Ashton-u-Lyne, Lancs OL5 9NU

1/32 scale F-4E(S) Peace Jack decals

from Cutting Edge sheet CED32001. Martin Gustafsson ☎ Uppgardsvagen 58, 44443, Stenungsund, Sweden ☎ galaxy106422@hotmail.com

Foreign (Greek, Turkish, Yugoslav)

Spitfire kits diecast/plastic, decals 1/72 and 1/48 scale, photos, others considered. Ian ☎ 174 Rowntree Avenue, York

HELP

I wish to contact Gunther Sengfelder,

who did the scale drawings for the book Bf-109 F-K by Radinger and Otto. Kenneth Race ☎ 906 Liberty Court, Cupertino, CA 95014, USA

I need 1/72 Mirage underwing/

fuselage stores: drop tanks (any size), JL 100 rocket pod/fuel tanks, Barax and Barracuda ECM pods and Magic, R511 and R530 AAMs. Also drawings of Soulé towed gunnery target. Contact Andy Kime ☎ 01628 487668 to arrange bits swap

Is anyone aware of any 1/72 scale

Messerschmitt Bf 109E kit that has the leading-edge slats and flaps deployed (or an add-on set)? I am planning to build some Battle of Britain versions but I don't want to have to indulge in lengthy cutting and trimming of kits. Peter Fawke ☎ 59 Paynesfield Road, Tatsfield, nr Westerham, Kent TN16 2BG ☎ peterfawke@aol.com

Could someone please send me

photocopies of the instructions and paint/decal guide for the Revell-ESCI A-4M/N Skyhawk. Stephen Nolan ☎ 70 Hillbrook Estate, Tullow, Co Carlow, Ireland ☎ stephennolan@oceanfree.net

PEN PALS

Grumpy old modeller

looking to write to those with interests in WW2 aviation. I started in 1947. Tony Rice ☎ Casa Mariposa, Macapa 7, 29711 Alcaucin, Malaga, Spain ☎ tony_ursula@yahoo.co.uk

My interests are early aviation

through to the golden age of the 30s but mostly 1914-18 era. If your interests are similar then please write to me. Graham ☎ 22 Innisfayre Gardens, Belfast, BT15 4GJ

I collect everything about Japanese

military airplanes — kits, literature, plans, special projects. I would like to correspond with Japanese modeller. Josef Hyzik, Czech Republic ☎ Hyzikj@seznam.cz

SWAP

1/48 Fonderie Miniature Ventura

multi-media kit for Airfix Lightning or 1/32 scale Revell Hunter. 1/48 Pro-Modeler Focke-Wulf Fw 190G-2 for Eduard Fokker D.VII. P Robinson ☎ 2 North End Cottis, Hilston Road, Toos, Hull HU12 0JA ☎ 01964 670492

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Scale Aviation Modeller International will advertise forthcoming events in this column free of charge for event organisers who wish to give advance notice of their shows. We will continue to run your notice until the event is held, space permitting.



Till November

Unforgotten Falklands

Exhibition to mark the 25th Anniversary of the Falklands War. FAA Museum, RNAS Yeovilton, Somerset ☎ 01935 842638
marketing@fleetairarm.com



Nationals

AUGUST

Wed 22nd – Saturday 25th
IPMS USA National Convention
Anaheim, California. www.ipmsusa.org

NOVEMBER

Saturday 10th – Sunday 11th
IPMS UK Scale Model World
Telford International Centre

This service is offered free to all our readers. Send adverts to:

Events
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AUGUST

August 5th

Great Midlands Kit Swap

St Mary's Hall, Hinckley, Leicestershire. Contact Phil Middleton ☎ 01162 867107

August 12th

Aeroventure Model Show

Doncaster AeroVenture Air Museum, Dakota Way, Airborne Road, Doncaster Leisure Park. Entry fee includes entry into the Museum. Martin or Dee ☎ 01302 761616 or 07990 764364

August 19th

Airshow, Collectors' Fair and Fly-In

Redhill Aerodrome, Surrey
Aviation, Military, Book and Model Collectors' stands. Free car parking. ☎ 01737 822200
www.redhillairshow.co.uk

August 22nd – 25th

IPMS USA National Convention 2007

Anaheim Marriott Resort Hotel, Orange County, California ☎ information@ipmsusa2007.org
www.ipmsusa2007.org

August 25th – 26th

Shackleton 50th Anniversary Event

Celebrating the 50th Anniversary of the Avro Shackleton MR.3 entering RAF service, with former Shackleton aircrew and groundcrew participating. Newark Air Museum, Winthorpe, Nottinghamshire. ☎ www.newarkairmuseum.co.uk

SEPTEMBER

September 8th

9th Annual Air Zoo Model Show

IPMS/Kalamazoo Scale Modelers and IPMS West Michigan. Air Zoo Museum, 6151 Portage Road, Kalamazoo, Michigan, USA. Contact Clare Wentzel ☎ (269) 353 6302 ☎ cewentzel@yahoo.com.

September 9th

Wombourne Aerospace & Vehicle Club Scale Model Show and Competition

The Community Centre, Church Rd, Wombourne 10am to 4pm. For info send SAE to G Taylor ☎ 11 Holberg Grove, Wolverhampton WV11 3LE or J Turner ☎ 340 Stourbridge Rd, Brierley Hill DY5 1JA or ☎ www.communitate.co.uk/bc/asvc/index.phtml

September 16th

Sutton Coldfield Model Spectacular

Sutton Coldfield Town Hall, West Midlands 10.00 a.m. to 4.30 p.m. Traders contact Paul Grimley ☎ 01543 481428. Clubs contact Peter Haywood ☎ 01889 578074

September 16th – 17th

IPMS Germany Modelling Show

Luftwaffe Museum, Berlin-Gatow. 9.00 a.m. to 5.00 p.m. Free entrance. Contact: Frank Schirmer, Landsberger Allee 98, 10249 Berlin, Germany ☎ +49 30 233 626 90, Fax +49 30 351 045 85 ☎ Ausstellung@ipmsdeutschland.de
www.ipmsdeutschland.de

September 22nd

Aerojumble and Fly-In

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Flying Legends – Duxford, 2007

September 22nd

IPMS West Cornwall Model Show

St. Johns Ambulance Hall, Penzance. Anyone interested can contact Arthur Johnson ☎ 01736757726 or Mark Wardley ☎ 01209214691
markwardley1961@yahoo.co.uk

September 29–30th

IPMS Belgium National Convention

The Oktoberhallen in Wieze, about 20 km away from Brussels. Full details about the exhibition, contest and sales stands including registration forms are available at ☎ www.ipms.be or ☎ http://ipms.be/NC2007/NatCon2007Page.htm or write to ☎ IPMS Belgium, Esdoornlaan 33, B-1850 Grimbergen

September 30th

IPMS Brampton 2007 Show

New, improved venue: The Burgess Hall, St. Ives, Cambridgeshire. Adults £1.50, Concessions 50p. Alec or Charlie ☎ 01480 896949, Dave ☎ 01480 861387 ☎ bramptonshow@hotmail.co.uk
www.bramptonmodelclub.fsnet.co.uk

OCTOBER

October 7th

IPMS Stratford Igor Sikorsky Chapter Model Show

The 16th annual event takes place at the Knights of Columbus Hall, 625 Bridgeport Avenue, Milford CT. See www.ipmsstratford.org or contact Andy Petrahai ☎ ipmsstratford@yahoo.org or Ray Prokoski ☎ Raymond.prokoski@snet.net

October 12th – 13th

IPMS-USA Region 2 Convention

Sponsored by IPMS Roanoke Valley and the 'VA Shoot-Out', Roanoke Civic Center Exhibition Hall, Roanoke, Virginia, USA. Club displays, Over 90 vendor tables, 'make 'n' take for kids. Contact Tim Ward ☎ twardf86@cox.net ☎ www.rvipsms.com

October 13th

Glasgow Open Model Show & Comp

Glasgow IPMS and Miniature Armour Group Show, Kelvin Hall International Sports Arena, Argyle Street, Glasgow. Forty-two competition classes, 20 traders, 20 club and SIG displays. Contact Geoff Crow ☎ geoff.crow@east-ayrshire.gov.uk or ☎ geoff.crow@btinternet.com

October 20th

Fleet Air Arm Museum's Autumn Model Show

With emphasis on attracting younger model enthusiasts, admission will be free to children when accompanied by a full paying adult. There will be competitions, workshops and talks where model enthusiasts of all levels will find plenty to interest and motivate them ☎ www.fleetairarm.com ☎ 01935 840565

October 20th

Model & Aviation Collectors' Show

Fleet Air Arm Museum, RNAS Yeovilton, Somerset. 10.30 am. ☎ 01935 840565

2008

FEBRUARY

February 3rd

ModelKraft 2008

New venue! Stantonbury Leisure Centre, Milton Keynes, Buckinghamshire ☎ www.mksmc.co.uk

ModelKraft 2008



Due to redevelopment work in Bletchley it will not be possible to hold ModelKraft 2008 at our usual venue, but we have booked

Stantonbury
Leisure Centre
Milton Keynes

for the 3rd February 2008.
For up to date information see our web site www.mksmc.co.uk



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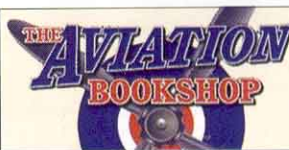
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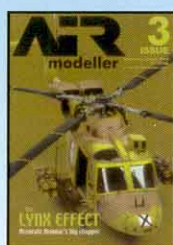
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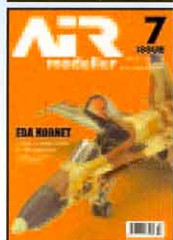
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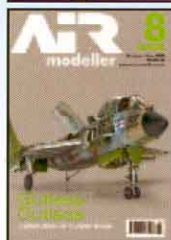
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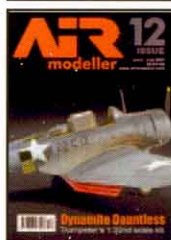
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